

FED. ROAD DW. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
N/A	RI	N/A	2024	1	20

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# RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY



## BRIDGE REPAIRS

### JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE FISHING PIER ACCESS ROAD BRIDGE NO. 083901

TOWN OF NORTH KINGSTOWN  
WASHINGTON COUNTY

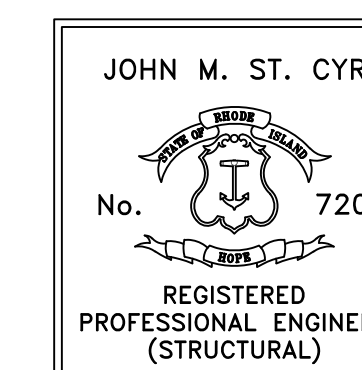
CONTRACT NO. 24-06



#### R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS

SPECIFICATIONS TO GOVERN THIS PROJECT ARE THE R.I. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, FEBRUARY 2025, WITH ALL REVISIONS AND THE STATE AND FEDERAL SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD DETAILS, 1998 EDITION, WITH ALL REVISIONS.



Contract Number 24-06  
 Number of Sheet 1  
 Total Sheets 20



RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY

ACCEPTED BY: \_\_\_\_\_ DATE \_\_\_\_\_

ERIC SEABURY, P.E.  
 DIRECTOR OF ENGINEERING  
 RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY  
 JAMESTOWN, RHODE ISLAND

RECOMMENDED FOR APPROVAL

*John M. St. Cyr* \_\_\_\_\_ DATE 4/2/2025

JOHN ST. CYR, P.E.  
 GM2 ASSOCIATES, INC.  
 PAWTUCKET, RHODE ISLAND

## LIST OF ABBREVIATIONS

### A

ABUTMENT = ABUT.  
 ADDITIONAL = ADD'L  
 ALTERNATE = ALT.  
 ANCHOR BOLT AND = A.B.  
 APPROACH = &  
 APPROVED = APPR.  
 APPROXIMATE = APPD.  
 AT EACH = APPROX.  
 AVENUE = @  
 AVERAGE = AVE.  
 = AVG.

### B

BACK TO BACK = B TO B  
 BASELINE = B  
 BEAM = BM.  
 BETWEEN = BTWN.  
 BEARING = BRG.  
 BITUMINOUS = BIT.  
 BUILDING = BLDG.  
 BUILDING LINE = B.L.  
 BOLT CIRCLE = B.C.  
 BOTTOM = BOT.  
 BOTTOM OF = B.O.

### C

CAST-IN-PLACE = C.I.P.  
 CENTER TO CENTER = C TO C  
 CENTERLINE = C  
 CIRCLE = CIR.  
 CLEARANCE = CLR.  
 COLUMN = COL.  
 CONCRETE = CONC.  
 CONDUIT = COND.  
 CONNECTION = CONN.  
 CONSTRUCTION = CONST.  
 CONTRACTION = CONTR.  
 COUNTERSINK = CSK.  
 COUPLING = CPLG.  
 CLASS I CONTROLLED LOW STRENGTH MATERIAL = CLMS  
 CUBIC FEET = CF

### D

DETAIL = DET.  
 DIAGONAL = DIAG.  
 DIAPHRAGM = DIAPHM.  
 DIAMETER = DIA. OR Ø  
 DIMENSION = DIM.  
 DOWN = DN.  
 DRAWING = DWG.  
 DRAIN = DR.

### E

EACH = EA.  
 EACH FACE = EF  
 EAST = E.  
 ELEVATION = EL. OR ELEV.  
 EMBEDMENT = EMBED.  
 EXISTING = EXIST.  
 EXPANSION = EXP.  
 EQUAL = EQ.

### F

FAR FACE = FF  
 FAR SIDE = FS  
 FABRICATE = FAB.  
 FACE TO FACE = F TO F  
 FEET = FT.  
 FLANGE = FLG.  
 FLAT HEAD = F.H.  
 FOOTING = FTG.  
 FORCE MAIN = FM  
 FOUNDATION = FDN.  
 FURNISH, FABRICATE & ERECT = F.F.&E.

### G

GAGE = GA.  
 GALVANIZE = GALV.  
 GAS = G  
 GRADE = GR.  
 GRATING = GRTG.  
 GROUND = GND.

### H

HEIGHT = HGT.  
 HEXAGON = HEX.  
 HIGHWAY = HWY.  
 HIGH STRENGTH = HS  
 HORIZONTAL = HORIZ.

### I

INCH = IN.  
 INFORMATION = INFO.  
 INSIDE DIAMETER = I.D.  
 INVERT = INV.

### J

JOINT = JT.

### L

LENGTH = LGTH. OR L  
 LENGTH OF VERTICAL CURVE = LVC  
 LEFT = LT.  
 LIGHTING = LTG.  
 LONG = LG.  
 LOAD & RESISTANCE FACTOR DESIGN = LRFD

### M

MATERIAL = MATL.  
 MAXIMUM = MAX.  
 MEAN HIGH WATER = M.H.W.  
 MEAN LOW WATER = M.L.W.  
 MEAN SEA LEVEL = M.S.L.  
 MECHANICAL = MECH.  
 MINIMUM = MIN.  
 MISCELLANEOUS = MISC.

### N

NEAR FACE = NF  
 NEAR SIDE = NS  
 NORTH = N.  
 NOT TO SCALE = NTS  
 NUMBER = NO. OR #

### O

OBSERVED WATER = O.W.  
 ON CENTER = OC  
 OPENING = OPNG.  
 OUTSIDE DIAMTER = O.D.  
 OPTIONAL = OPT.  
 OVERHEAD WIRES = O.H.W.

### P

PLATE = PL  
 PLUS OR MINUS = ±  
 POINT OF CURVATURE = PC  
 POINT OF VERTICAL CURVATURE = PVC  
 POINT OF VERTICAL INTERSECTION = PVI  
 POINT OF VERTICAL TANGENCY = PVT  
 POINT OF TANGENCY = PT  
 POLYVINYL CHLORIDE = PVC  
 POUNDS = LBS.  
 POUNDS PER SQUARE INCH = PSI  
 POUNDS PER SQUARE FOOT = PSF  
 PRESTRESSED PRECAST CONCRETE = P.P.C.  
 PRECAST CONCRETE = P.C.

### R

RADIUS = RAD. OR R  
 RAILROAD = RR  
 REQUIRED = REQ'D.  
 REINFORCING = REINF.  
 REHABILITATION = REHAB.  
 REMOVE & DISPOSE = R&D  
 RIGHT = RT.

### S

SECTION = SECT.  
 SCHEDULE = SCH.  
 SCHEMATIC = SCHEM.  
 SHEET = SHT.  
 SIDEWALK = SDWK.  
 SOUTH = S.  
 SPACES = SP.  
 STANDARD = STD.  
 STATION = STA.  
 SYMMETRICAL = SYM.  
 STAY IN PLACE = S.I.P.  
 SQUARE = SQ.

### T

TOP = T  
 TOP AND BOTTOM = T&B  
 TOP OF = T.O.  
 THICK = THK.  
 TYPICAL = TYP.

### U

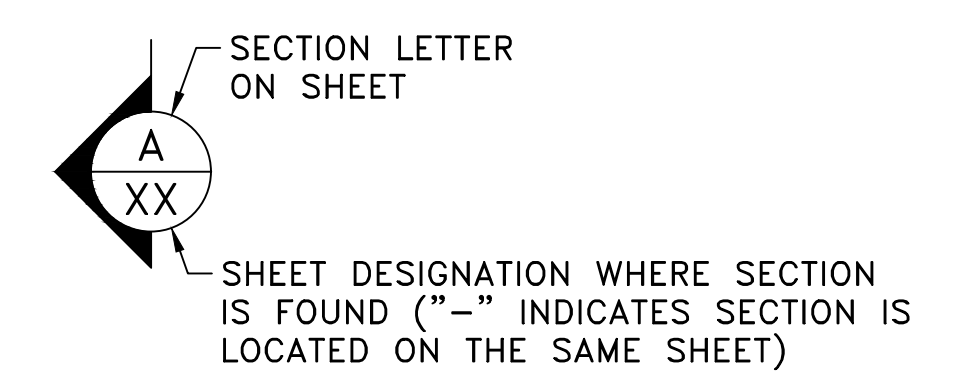
UNLESS NOTED OTHERWISE = U.N.O.

### V

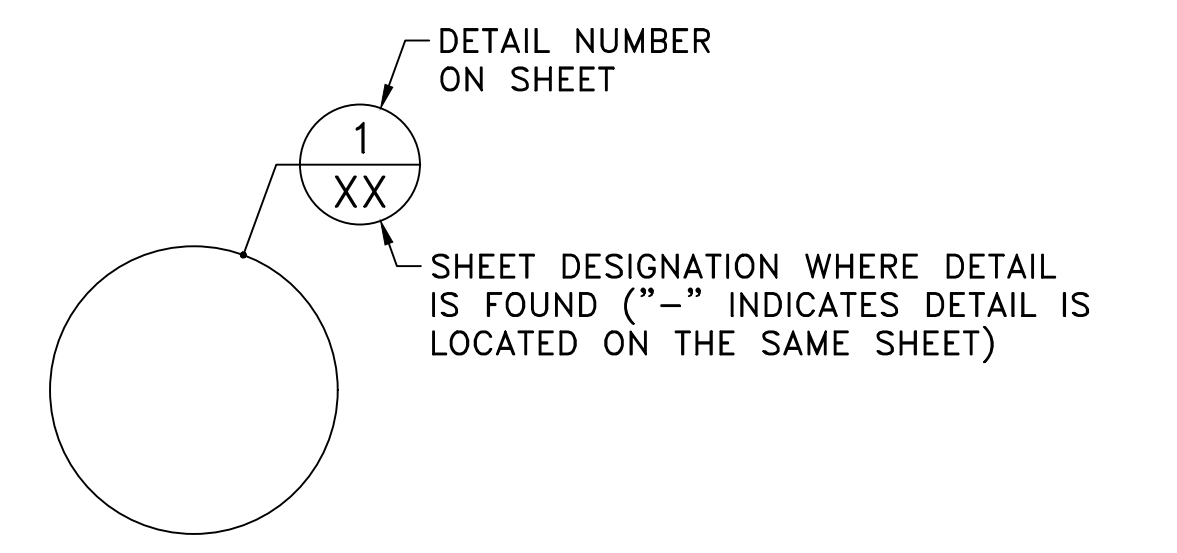
VARIES = VAR.  
 VERTICAL CURVE = V.C.  
 VERTICAL = VERT.

### W

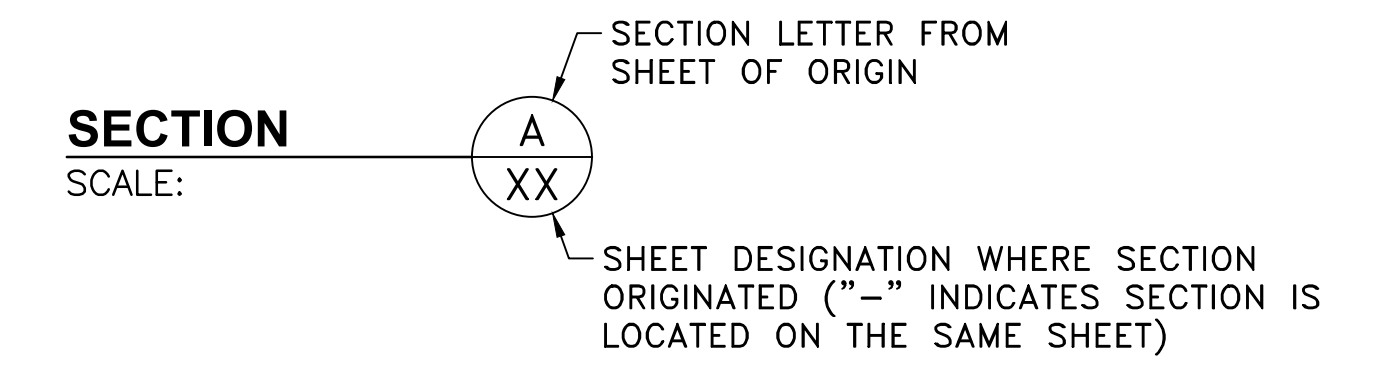
WATER = W  
 WELDED WIRE FABRIC = W.W.F.  
 WEST = W.  
 WITH = W/  
 WIDE FLANGE = W.F.  
 WORKING POINT = W.P.



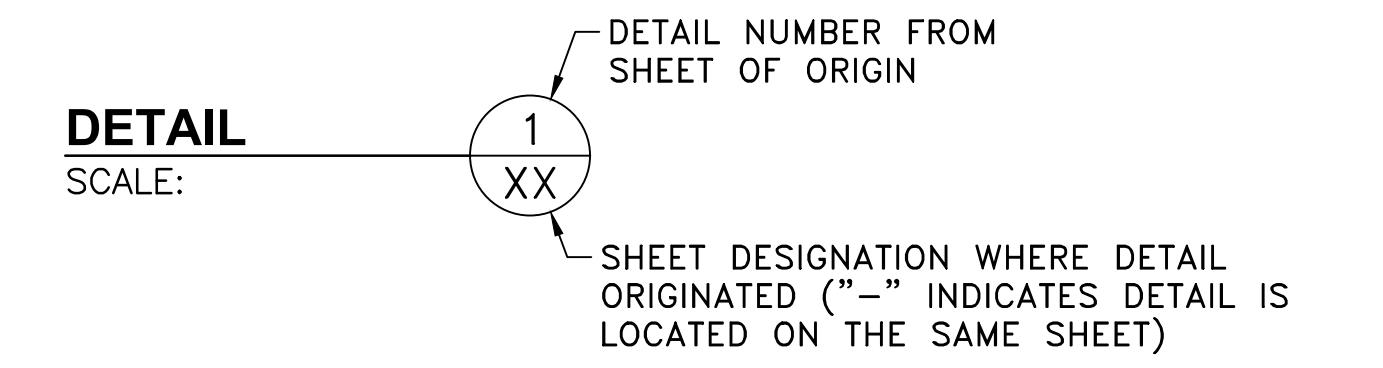
**SECTION MARK**



**DETAIL MARK**



**SECTION TITLE**



**DETAIL TITLE**

## SECTION & DETAIL DESIGNATIONS

		<p><b>RHODE ISLAND</b> TURNPIKE AND BRIDGE AUTHORITY</p>	<p>DESIGNED BY: CHECKED BY: DATE: SHEET: OF:</p>	<p>SCALE:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th colspan="3">REVISIONS</th> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>NO.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	REVISIONS			REVISIONS			NO.	DATE	BY	NO.	DATE	BY																			<p><b>BRIDGE REPAIRS</b> JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE FISHING PIER ACCESS ROAD BRIDGE NO. 083901 NORTH KINGSTOWN RHODE ISLAND</p> <p><b>LIST OF ABBREVIATIONS</b></p>
REVISIONS			REVISIONS																																
NO.	DATE	BY	NO.	DATE	BY																														

### GENERAL NOTES

- ALL CONSTRUCTION INDICATED ON THESE PLANS SHALL BE IN ACCORDANCE WITH:
  - THE LATEST EDITION OF AND SUPPLEMENTS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).
  - THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) LRFD BRIDGE CONSTRUCTION SPECIFICATIONS, NINTH EDITION, 2020, INCLUDING THE LATEST INTERIM REVISIONS.
  - THE SPECIFICATIONS ACCOMPANYING THESE PLANS.
- DIMENSIONS, STATIONS, AND ELEVATIONS ARE SHOWN TO THE NEAREST ONE-HUNDREDTH OF A FOOT OR ONE-EIGHTH OF AN INCH, EXCEPT STRUCTURAL STEEL DIMENSIONS WHICH ARE TO THE NEAREST ONE-SIXTEENTH OF AN INCH.
- ANGLES ARE SHOWN TO THE NEAREST SECOND.
- ALL WORKING POINTS ARE SHOWN AT THE CENTERLINES OF BEARINGS OF ABUTMENTS AND AT THE CENTERLINES OF PIERS, UNLESS OTHERWISE NOTED.
- DIMENSIONS, ANGLES, AND LAYOUT OF THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE ORIGINAL CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY ALL ELEVATIONS, DIMENSIONS, DETAILS, ANGLES, STRUCTURAL MEMBER SIZES, AND LAYOUTS AS SHOWN ON THESE PLANS. FIELD CONDITIONS MAY EXIST WHICH DEVIATE FROM THE INFORMATION SHOWN ON THESE PLANS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR FABRICATION AND FIT OF HIS WORK.
- THE EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE LOCATED USING THE BEST AVAILABLE INFORMATION. NO BUILDING SERVICE CONNECTIONS (ELECTRIC, TELEPHONE, GAS, WATER, SANITARY AND OTHERS) ARE SHOWN. THE CONTRACTOR IS TO ASSUME THAT SERVICES TO ALL BUILDINGS ARE PRESENT.
- BOTH FEDERAL AND STATE LAW (RI. GENERAL LAW 39-1.2) REQUIRE NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BLASTING, DEMOLISHING, BORING, BACK FILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE STATE AND/OR THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE STATE.

### DESIGN DATA

#### DESIGN SPECIFICATIONS

- THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, NINTH EDITION, 2020, INCLUDING ALL INTERIM REVISIONS TO DATE.
- THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL 2007 EDITION INCLUDING ALL REVISIONS TO DATE.
- ALL OTHER APPLICABLE DESIGN SPECIFICATIONS ARE REFERENCED IN SECTION 1 OF THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL DATED 2007.
- THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).
- IN CASE OF CONFLICT, THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL SHALL GOVERN.

### MATERIALS

#### STRUCTURAL STEEL:

- AASHTO DESIGNATION M 270, GRADE 36 MIN.

#### REINFORCING STEEL:

- AASHTO DESIGNATION M 31, GRADE 60, GALVANIZED

#### CONCRETE STRENGTHS:

- CLASS HP 3/4" f<sub>c</sub> = 5,000 PSI (28 DAYS)

### CONCRETE NOTES

- CLASS OF CONCRETE SHALL BE HIGH PERFORMANCE CLASS HP AS DESCRIBED IN THE RI STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS OF THE SPECIFICATIONS.
- THE CONTRACTOR MAY, AT THE APPROVAL OF THE ENGINEER, PROPOSE THE USE OF SELF CONSOLIDATING CONCRETE FOR ANY CLASS OF CONCRETE ON THIS PROJECT.
- ALL PORTLAND CEMENT CONCRETE SHALL BE AIR-ENTRAINED.
- ALL REINFORCING STEEL SHALL BE GALVANIZED. ALL WIRE TIES AND MISCELLANEOUS HARDWARE USED FOR PLACEMENT OF GALVANIZED REINFORCING SHALL ALSO BE GALVANIZED. GALVANIZED COATING FOR REINFORCING STEEL SHALL CONFORM TO ASTM DESIGNATION A 767, CLASS 1.
- ALL CRITICAL LAP SPLICES SHALL BE AS SHOWN ON THE PLANS. ALL SPLICES NOT SHOWN ON THE PLANS SHALL BE LAPPED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS FOR CLASS B LAP SPLICES.
- THE TOP BARS IN THE DECK SLABS SHALL BE SPLICED AT THE CENTER OF SPANS BETWEEN GIRDERS. THE BOTTOM BARS SHALL BE SPLICED OVER THE GIRDERS.
- UNLESS OTHERWISE INDICATED ON THE PLANS, ALL MAIN REINFORCING BARS SHALL HAVE THE FOLLOWING MINIMUM COVER:

	MINIMUM COVER
CONCRETE CAST AGAINST OR PERMANENTLY EXPOSED TO EARTH (FOOTINGS, ABUTMENT AND WALL FACES, BACKWALLS)	3"
DECK SLAB	TOP 2" (+1/4", -0") BOTTOM 1" (+1/8", -0")
ALL OTHER BARS	2"
- COVER TO TIES AND STIRRUPS MAY BE 1/2" LESS THAN THE ABOVE VALUES SPECIFIED FOR MAIN REINFORCING, BUT IN NO CASE LESS THAN 1/2".
- HORIZONTAL CONSTRUCTION JOINTS OTHER THAN THOSE SHOWN ON PLANS WILL NOT BE PERMITTED WITHOUT A WRITTEN REQUEST BY THE CONTRACTOR AND PRIOR AUTHORIZATION BY THE ENGINEER.
- UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED NEW CONCRETE SURFACES VISIBLE IN ELEVATION TO ONE FOOT BELOW FINAL GROUND LINE (AND THE UNDERSIDE OF ALL CONCRETE DECK SLABS OUTSIDE OF THE FASCIA BEAM) SHALL RECEIVE A CONCRETE SURFACE RUBBED FINISH IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS.
- ALL EXPOSED EDGES AND REENRANT CORNERS NOT OTHERWISE DETAILED ON THE PLANS SHALL HAVE A MINIMUM 1/4" CHAMFER.
- ALL JOINT SEALANT SHALL BE POLYURETHANE ELASTOMERIC OR SILICONE SEALANT AS DESIGNATED ON THE PLANS. THE COLOR OF THE JOINT SEALANT, WHERE EXPOSED, SHALL BE NEUTRAL (LIGHT GRAY OR TAN). COLOR OF THE SEALANT, WHERE NOT EXPOSED, WILL BE AT THE DISCRETION OF THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING CONCRETE STAINS OR DISCOLORATION DURING CONSTRUCTION UNTIL SUCH TIME AS THE SURFACES ARE APPROVED AND ACCEPTED. ANY CONCRETE STAINS OR DISCOLORATION OCCURRING PRIOR TO ACCEPTANCE OF THE SURFACES SHALL BE REMOVED BY THE CONTRACTOR AT AT NO ADDITIONAL COST TO THE STATE.
- UNLESS OTHERWISE NOTED ON THE PLANS JOINT FILLER IS TO BE PREFORMED, NON-EXPANSIVE, NON-EXTRUDING TYPE IN ACCORDANCE WITH SECTION M.02.10.1 OF THE RI STANDARD SPECIFICATIONS.
- ALL DECK FORMS SHALL BE OF THE REMOVABLE TYPE THAT WILL PRODUCE THE DIMENSIONS SHOWN ON THE PLANS.
- IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS, ALL METAL TIES OR ANCHORAGES WHICH ARE REQUIRED FOR CONCRETE FORMWORK SHALL BE SO CONSTRUCTED THAT THEY CAN BE REMOVED TO AT LEAST TWO INCHES BELOW THE EXPOSED SURFACE OF THE CONCRETE WITHOUT CAUSING DAMAGE TO THE CONCRETE SURFACE AND SHALL BE GALVANIZED. SNAP TIES MAY BE USED ONLY IF APPROVED BY THE ENGINEER. IF THE CONTRACTOR PROPOSES TO USE THEM, A CATALOG CUT AND OTHER NECESSARY INFORMATION MUST BE SUBMITTED TO THE ENGINEER TO DEMONSTRATE THAT THE TIES WILL SNAP-OFF FAR ENOUGH INTO THE CONCRETE TO ALLOW FOR PROPER PATCHING. SNAP TIES MUST PROVIDE ADEQUATE STRENGTH TO SUPPORT THE FORMS. ALL CAVITIES SHALL BE FILLED WITH AN APPROVED CEMENT MORTAR MEETING THE REQUIREMENTS OF ASTM C 928.

### STRUCTURAL STEEL NOTES

- FRAMING DIMENSIONS ARE GIVEN ALONG CENTERLINES OF BEAMS AND ALONG CENTERLINES OF BEARINGS AT ABUTMENTS. THE FABRICATOR IS RESPONSIBLE FOR INCORPORATING THE CAMBER, CROSS SLOPE, AND OTHER EFFECTS THAT MAY IMPACT THE OVERALL LENGTHS, DIMENSIONS AND/OR THE DETAILING.
- THE SHOPS FABRICATING THE STRUCTURAL STEEL SHALL AT A MINIMUM BE CERTIFIED FOR "SIMPLE STEEL BRIDGES (SBR)" IN ACCORDANCE WITH THE AISC QUALITY CERTIFICATION PROGRAM OR EQUIVALENT. THE SHOPS SHALL ALSO BE CERTIFIED UNDER THE AISC "SOPHISTICATED PAINT ENDORSEMENT (SPE)" QUALITY PROGRAM OR THE SSPC-QP3 PAINT CERTIFICATION PROGRAM. THE FABRICATOR MUST SUBMIT PROOF OF CURRENT CERTIFICATION AS SPECIFIED.
- THE STEEL ERECTOR/CONTRACTOR FOR THIS PROJECT SHALL AT A MINIMUM BE CERTIFIED FOR "CERTIFIED STEEL ERECTOR (CSE)" IN ACCORDANCE WITH THE AISC QUALITY CERTIFICATION PROGRAM. THE ERECTOR/CONTRACTOR OF THE STRUCTURAL STEEL SHALL BE REQUIRED TO SUBMIT PROOF OF CURRENT CERTIFICATION AS SPECIFIED.
- SHOP DRAWINGS FOR ALL FABRICATED STEEL SHALL BE SUBMITTED TO THE ENGINEER IN SUFFICIENT TIME TO PERMIT CAREFUL CHECKING PRIOR TO FABRICATION.
- STRUCTURAL STEEL SHAPES AND PLATES SHALL CONFORM TO THE LATEST PROVISIONS OF AASHTO DESIGNATION M 270 GRADE 36 MINIMUM.

**NONFRACTURE-CRITICAL**

GRADE 36	15 FT-LBS @ 40°F (UP TO 4 INCHES THICK)
GRADE 50 OR 50W	15 FT-LBS @ 40°F (UP TO AND INCLUDING 2 INCHES THICK)
GRADE 50 OR 50W	20 FT-LBS @ 40°F (FROM 2 INCH THICK UP TO AND INCLUDING 4 INCHES THICK)

SAMPLING AND TESTING PROCEDURES SHALL BE IN ACCORDANCE WITH AASHTO T 243. THE FREQUENCY OF TESTING SHALL BE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.
- UNLESS OTHERWISE NOTED, ALL HIGH STRENGTH BOLTS SHALL CONFORM TO ASTM DESIGNATION F 3125, GRADE A325, MECHANICALLY GALVANIZED, AND THEY SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 824 OF THE RI STANDARD SPECIFICATIONS.
- WASHERS MEETING ASTM DESIGNATION F 436 ARE TO BE USED OVER ALL HOLES THAT ARE MORE THAN 1/16" IN DIAMETER GREATER THAN THE BOLT DIAMETER AND UNDER ALL PARTS TURNED DURING ASSEMBLY.
- NEW STRUCTURAL STEEL SHALL BE PREPARED AND PRIMED IN THE SHOP IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS; PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE STEEL REPAIR. THE FINAL INTERMEDIATE AND TOP COAT SHALL BE APPLIED IN THE FIELD AND INCLUDED FOR PAYMENT UNDER ITEM CODE 825.9901.
- PRIOR TO FABRICATION, ALL MATERIALS SHALL BE BLAST-CLEANED TO AT LEAST SSPC-SP6 TO REMOVE ALL OIL, DIRT, GREASE, MILL SCALE AND OTHER DELETERIOUS MATERIALS FROM THE SURFACES OF THE STEEL TO BE FABRICATED.
- PRIOR TO SHOP COATING AS SPECIFIED IN SECTION 825 OF THE RI STANDARD SPECIFICATIONS, ALL CORNERS AND EDGES OF STEEL WHICH HAVE BEEN FLAME CUT OR OTHERWISE HARDENED SHALL BE SOFTENED BY GRINDING OR BLAST-CLEANING TO PROVIDE A SURFACE SUITABLE FOR APPLICATION OF THE SPECIFIED PAINT SYSTEM.
- UPON COMPLETION OF ALL FABRICATION AND PRIOR TO THE APPLICATION OF THE SHOP PRIMER COAT, THE STRUCTURAL STEEL SHALL BE RESTORED TO AN SSPC-SP10 CONDITION.

		<b>RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY</b>	DESIGNED BY: CHECKED BY: DATE: SHEET: OF:	SCALE:  <table border="1" style="font-size: 8px;"> <thead> <tr> <th colspan="3">REVISIONS</th> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>NO.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	REVISIONS			REVISIONS			NO.	DATE	BY	NO.	DATE	BY																			<b>BRIDGE REPAIRS</b> JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE FISHING PIER ACCESS ROAD BRIDGE NO. 083901 <small>NORTH KINGSTOWN RHODE ISLAND</small>
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**PAINTING STRUCTURAL STEEL NOTES**



1. THE LIMITS OF PAINTING STRUCTURAL STEEL SHALL BE AS NOTED WITHIN THE PLANS.
2. WITHIN THE LIMITS INDICATED, EXISTING STRUCTURAL STEEL TO BE REPAINTED SHALL INCLUDE, BUT NOT BE LIMITED TO, BEAMS, GIRDERS, BEARING ASSEMBLIES, CONNECTION PLATES, STIFFENERS, DIAPHRAGMS, CROSS FRAMES, AND FASTENERS.
3. STRUCTURAL STEEL SHALL BE PREPARED AND PAINTED IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS. THE COLOR OF THE TOP COAT SHALL BE BROWN TO MATCH FEDERAL 595A COLOR 10075.
4. NEW STRUCTURAL STEEL SHALL BE PREPARED AND PRIMED IN THE SHOP. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE STEEL REPAIR. THE INTERMEDIATE AND FINAL TOP COAT SHALL BE APPLIED IN THE FIELD AND INCLUDED FOR PAYMENT UNDER ITEM CODE 825.8040.
5. ALL EXISTING STRUCTURAL STEEL DESIGNATED TO BE REPAINTED SHALL BE CLEANED TO SSPC-SP10 "NEAR-WHITE BLAST CLEANING".
6. ALL EXISTING PAINT ON THE BRIDGES SHALL BE ASSUMED TO BE LEAD BASED PAINT.
7. ANY COMPONENT NOT DESIGNATED TO BE REPAINTED SHALL BE ADEQUATELY PROTECTED FROM CLEANING OPERATIONS.
8. ANY DAMAGE RESULTING FROM THE CONTRACTOR'S CLEANING AND PAINTING OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE STATE.
9. WORK ASSOCIATED WITH SETTING UP, MAINTAINING, AND REMOVING THE CONTAINMENT SYSTEM FOR THE SURFACE PREPARATION AND THE REPAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE CONDUCTED IN ACCORDANCE WITH THE PROPOSED TRAFFIC CONTROL PLANS AND THE TIME RESTRICTIONS SPECIFIED IN THE CONTRACT DOCUMENTS.

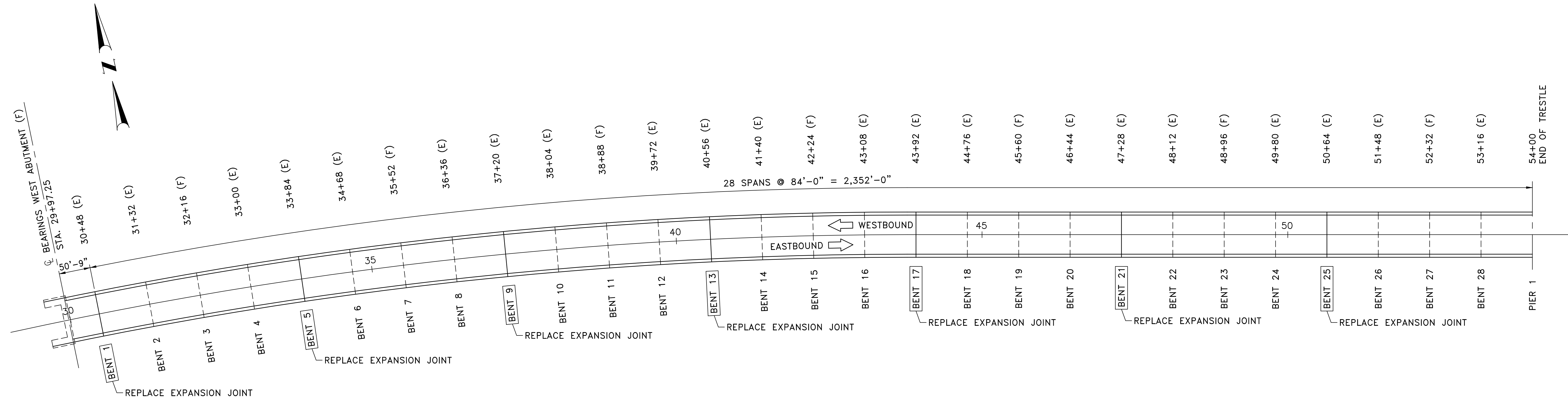
**MAINTENANCE AND PROTECTION OF TRAFFIC NOTES**

1. THE CONTRACTOR SHALL PERFORM ALL WORK WITHIN STRICT CONFORMANCE OF ALL MAINTENANCE OF TRAFFIC REQUIREMENTS AS SPECIFIED WITHIN THESE CONTRACT DOCUMENTS.
2. ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
3. ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE RIDOT STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
5. ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED, AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
6. ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
7. TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED.
8. PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.

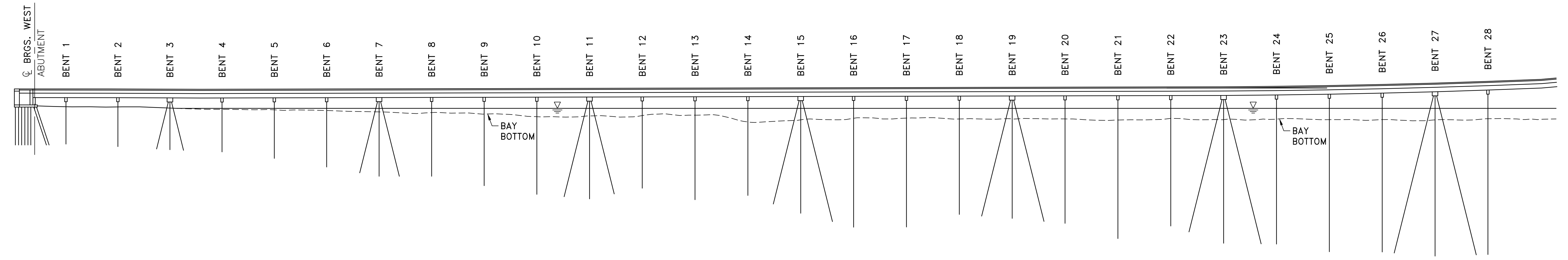
**CONSTRUCTION ACCESS, STAGING AND ENVIRONMENTAL REQUIREMENTS**

1. THE CONTRACTOR SHALL FURNISH THE RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY, AND THE ENGINEER, WITH A CPM PROGRESS SCHEDULE INDICATING THE ORDER IN WHICH THE WORK WILL PROGRESS, INCLUDING START AND FINISH DATES FOR WHICH THE VARIOUS WORK ITEMS WILL BE COMPLETED WITH MILESTONES. SEE SPECIFICATIONS.
2. DURING REMOVAL AND CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL NOT BE PERMITTED TO DROP MATERIAL OR DEBRIS INTO THE WATER FOR ANY REASON. ADEQUATE MEASURES SHALL BE TAKEN BY THE CONTRACTOR TO AVOID ANY ACCIDENTAL LOSS OF SUCH MATERIAL OR DEBRIS INTO THE THE WATER. IF THE ENGINEER DETERMINES THAT ADEQUATE MEASURES ARE NOT BEING IMPLEMENTED TO AVOID THE ACCIDENTAL LOSS OF MATERIAL OR DEBRIS INTO THE WATER, THE WORK SHALL BE SUSPENDED UNTIL SUCH INADEQUATE MEASURES HAVE BEEN CORRECTED. DELAYS RESULTING FROM THE ABOVE DESCRIBED SUSPENSION OF WORK WILL BE AT NO COST TO THE AUTHORITY.
3. ALL MATERIALS REMOVED FROM THE EXISTING STRUCTURE SHALL BE LEGALLY DISPOSED OF BY THE CONTRACTOR PER RHODE ISLAND DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM) AND COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC), AND ACCORDING TO ALL LOCAL, STATE, AND FEDERAL REQUIREMENTS AT THE CONTRACTOR'S OWN EXPENSE, OUTSIDE THE JURISDICTION OF THE RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY, UNLESS SPECIFICALLY NOTED OTHERWISE.
4. THE CONTRACTOR SHALL NOT BE PERMITTED TO STORE EQUIPMENT OR MATERIAL ON THE BRIDGE STRUCTURE WITHIN DAILY LANE CLOSURES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AN ADEQUATE MOBILIZATION AREA FOR THIS CONTRACT. SEE SPECIFICATION SECTION 936.
5. THE CONTRACTOR SHALL NOTIFY THE RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY 72 HOURS PRIOR TO THE ACTUAL START OF FIELD WORK AT THE BRIDGE SITE.
6. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT THEIR OPERATIONS IN SUCH A MANNER AS TO PRECLUDE POLLUTION OF THE BAY FROM DEBRIS, SEDIMENTS OR OTHER FOREIGN MATERIAL. ANY WATER THAT HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATION THAT CAUSES THE WATER TO BECOME POLLUTED WITH SAND, SILT, OIL OR OTHER IMPURITIES, SHALL NOT BE DISCHARGED INTO THE BAY.
7. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE INTO ACCOUNT THE CONDITION OF THE EXISTING STRUCTURE WHEN DETERMINING THE TYPE, SIZE AND WEIGHT OF ALL VEHICLES TO BE USED ON THE STRUCTURE DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT THIS INFORMATION ALONG WITH THE CONSTRUCTION LOADING ON THE BRIDGE TO THE ENGINEER FOR APPROVAL PRIOR TO USE.
8. UNLESS OTHERWISE NOTED IN THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB EXISTING UTILITIES, NAVIGATION LIGHTS, WEATHER SENSORS, INSTRUMENTATION, SECURITY CAMERAS, SIGNAGE, AND HIGHWAY LIGHTING. WHERE WORK AFFECTS OR IS AFFECTED BY EXISTING UTILITIES, THE WORK SHALL BE COORDINATED WITH THAT UTILITY COMPANY.
9. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ALL MATERIALS THAT ARE TO REMAIN IN PLACE, BE REUSED OR THAT ARE TO REMAIN THE PROPERTY OF RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS THAT ARE TO REMAIN IN PLACE, BE REUSED, OR THAT ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, THE DAMAGED MATERIALS SHALL BE REPLACED OR REPAIRED IN A MANNER SATISFACTORY TO AND APPROVED BY THE AUTHORITY AT NO ADDITIONAL COST TO THE AUTHORITY. THE CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING UTILITY LINE THAT IS DAMAGED BY THE CONSTRUCTION OPERATIONS TO THE SATISFACTION OF THE AUTHORITY AND THE AFFECTED UTILITY, AT NO ADDITIONAL COST. ANY PENALTIES BY THE UTILITY OWNERS DUE TO LOSS OF SERVICE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
10. THE CONTRACTOR SHALL PROVIDE THEIR OWN ELECTRICAL POWER SOURCE AND WATER SOURCE. THE CONTRACTOR SHALL NOT BE PERMITTED TO USE THE BRIDGE ELECTRICAL SYSTEM OR WATER SYSTEM.
11. THE CONTRACTOR SHALL PROVIDE SAFE ACCESS TO THE WORK AREA DURING CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ACCESS TO INSPECTORS, AUTHORITY PERSONNEL, AND THEIR DESIGNATED REPRESENTATIVES.
12. THE CONTRACTOR SHALL TAKE PRECAUTIONS SO THAT NO DEBRIS, MATERIALS, TOOLS, ETC. ARE LEFT ON THE ROADWAY SURFACE WHEN LEAVING THE WORK AREA. IN ADDITION, THE CONTRACTOR SHALL TAKE PRECAUTIONS SO THAT NO DEBRIS, MATERIALS, EQUIPMENT OR OPERATIONS INTERFERES WITH ADJACENT OPEN TRAFFIC LANES.
13. IT IS IMPERATIVE THAT THE WORK IS COMPLETED WITHIN THE ALLOTTED TIME PERIODS AS NOTED IN THIS CONTRACT IN ORDER TO RESTORE TRAFFIC AND OTHER AFFECTED SERVICES THAT ARE INTERRUPTED BY THE CONSTRUCTION WORK. THE CONTRACTOR SHALL TAKE THIS INTO ACCOUNT WHEN PREPARING THE PROGRESS SCHEDULE.

		<p><b>RHODE ISLAND TURNPIKE AND BRIDGE AUTHORITY</b></p>	DESIGNED BY: CHECKED BY: DATE: SHEET: OF:	SCALE: <table border="1" style="width:100%; border-collapse: collapse; text-align: center;"> <tr> <th colspan="3">REVISIONS</th> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>NO.</th> <th>DATE</th> <th>BY</th> </tr> <tr> <td> </td><td> </td><td> </td><td> </td><td> </td><td> </td> </tr> <tr> <td> </td><td> </td><td> </td><td> </td><td> </td><td> </td> </tr> </table>	REVISIONS			REVISIONS			NO.	DATE	BY	NO.	DATE	BY													<p><b>BRIDGE REPAIRS</b>          JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE          FISHING PIER ACCESS ROAD BRIDGE NO. 083901          NORTH KINGSTOWN RHODE ISLAND</p> <p><b>GENERAL NOTES - 2</b></p>
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**GENERAL PLAN**  
SCALE: 1" = 80'



**ELEVATION**  
SCALE: 1" = 80'

**DESCRIPTION OF PROPOSED REPAIR WORK:**

- REPLACEMENT OF EXISTING EXPANSION JOINTS AT BENTS 1, 5, 9, 13, 17, 21 AND 25 WITH HEAVY-DUTY POLYMER EXPANSION JOINTS.

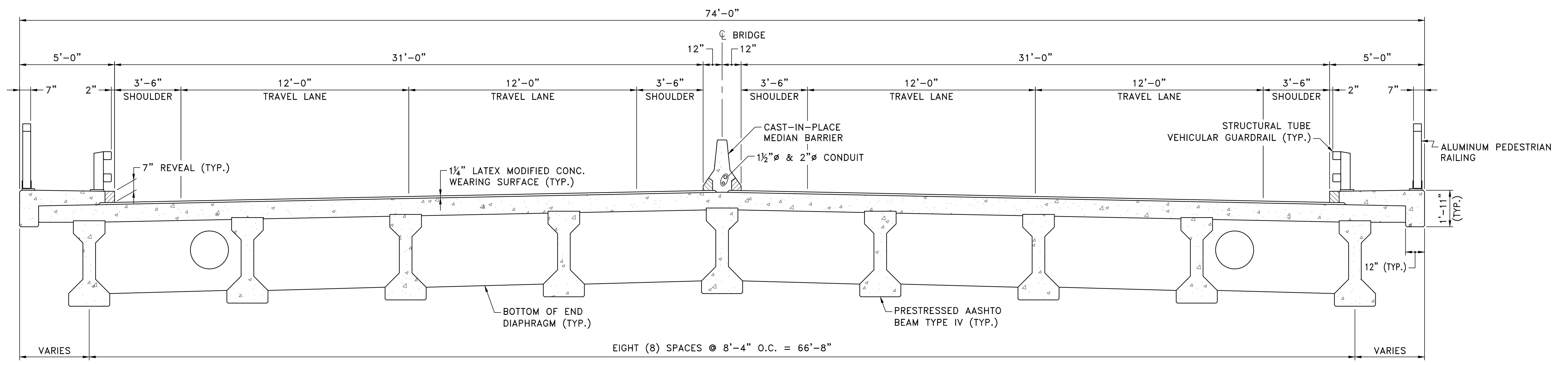


RHODE ISLAND  
TURNPIKE AND BRIDGE AUTHORITY

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**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**GENERAL PLAN & ELEVATION**



**TYPICAL TRANSVERSE SECTION AT BENT**  
 SCALE: 3/8" = 1'-0"



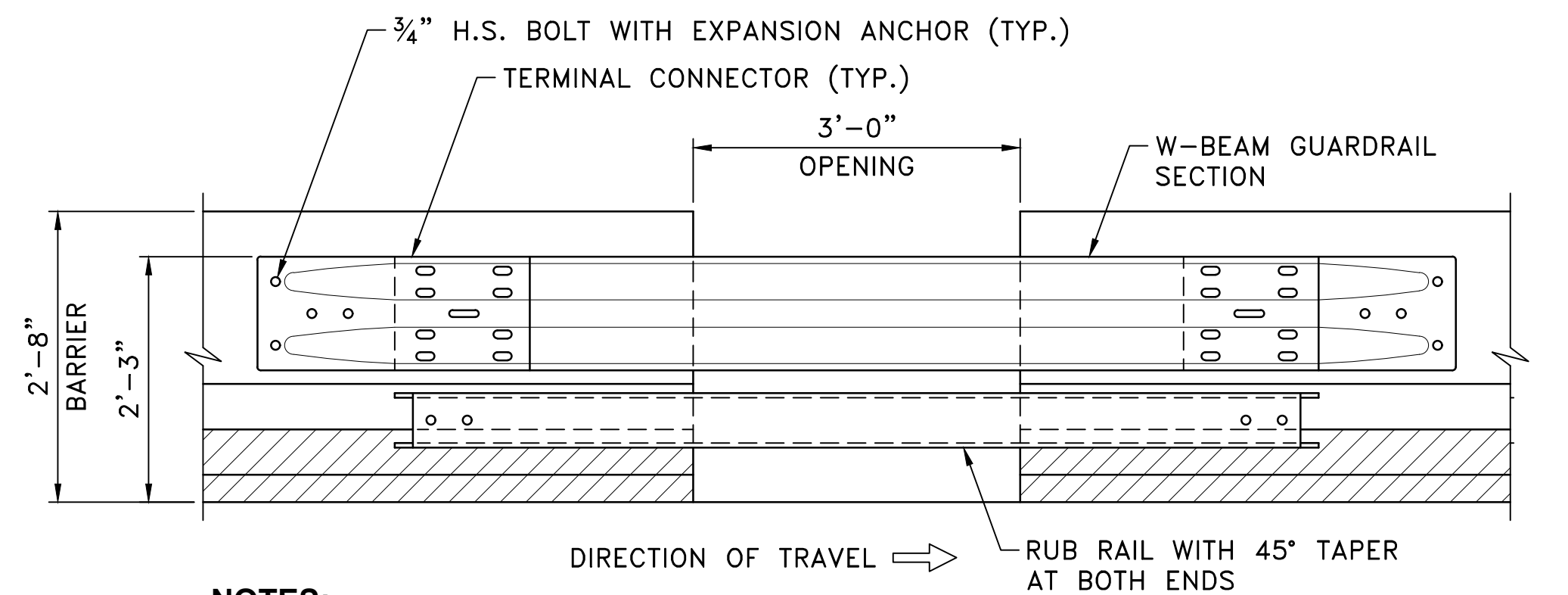
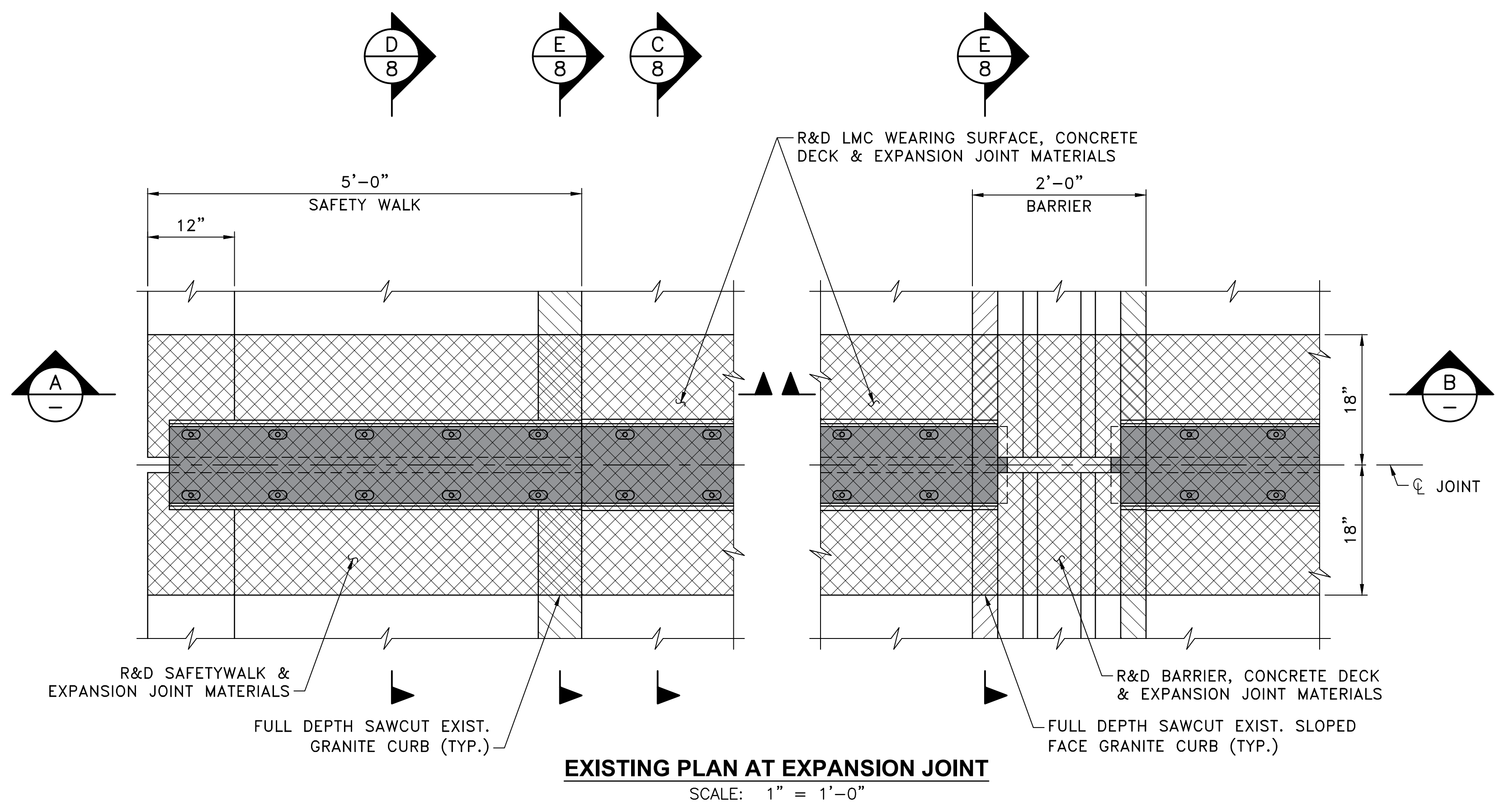
RHODE ISLAND  
 TURNPIKE AND BRIDGE AUTHORITY

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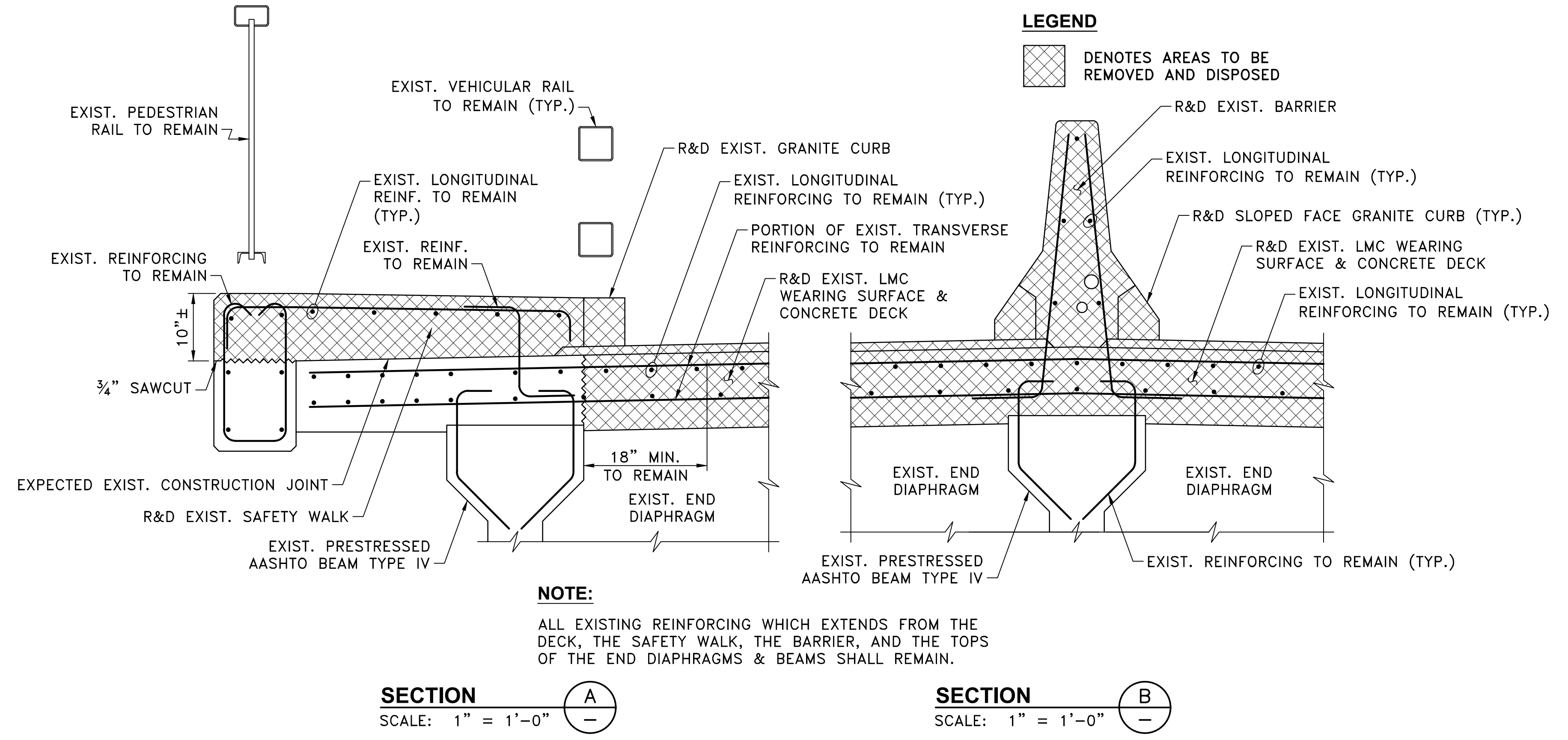
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**BRIDGE REPAIRS**  
 JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
 FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
 NORTH KINGSTOWN RHODE ISLAND  
 BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**TYPICAL SECTION**



- NOTES:**
- GUARDRAIL PROTECTION SHALL BE IN PLACE WHEN TRAFFIC IS ALLOWED ADJACENT TO THE BARRIER.
  - EXPANSION ANCHORS SHALL BE REPLACED AFTER REMOVING AND RE-INSTALLING ANCHOR BOLTS A MAXIMUM OF SEVEN (7) TIMES.
  - UPON COMPLETION OF THE WORK, THE EXPANSION ANCHORS SHALL BE REMOVED AND THE HOLES FILLED WITH NON-SHRINK GROUT TO THE SATISFACTION OF THE ENGINEER.

**MEDIAN BARRIER PROTECTION DURING CONSTRUCTION**  
SCALE: 3/4" = 1'-0"



**NOTE:**  
ALL EXISTING REINFORCING WHICH EXTENDS FROM THE DECK, THE SAFETY WALK, THE BARRIER, AND THE TOPS OF THE END DIAPHRAGMS & BEAMS SHALL REMAIN.

**SECTION A**  
SCALE: 1" = 1'-0"

**SECTION B**  
SCALE: 1" = 1'-0"



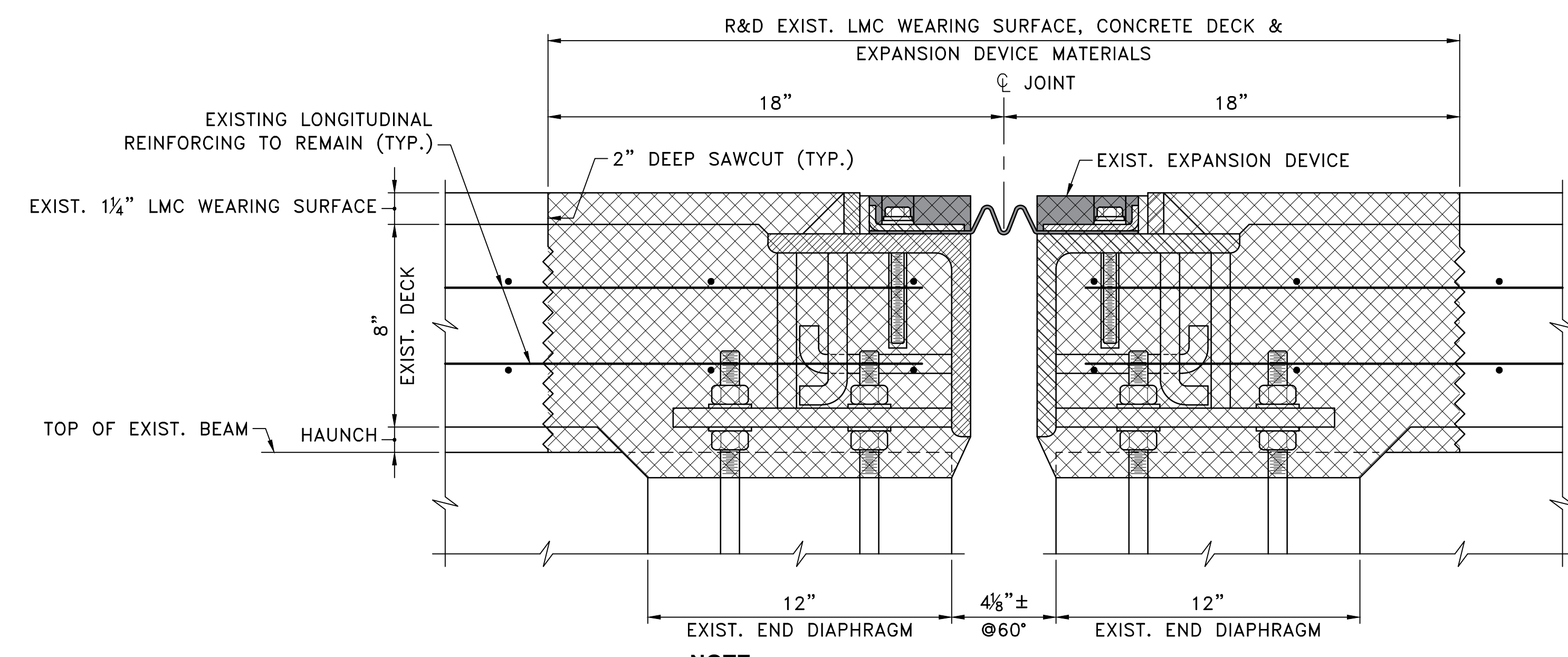
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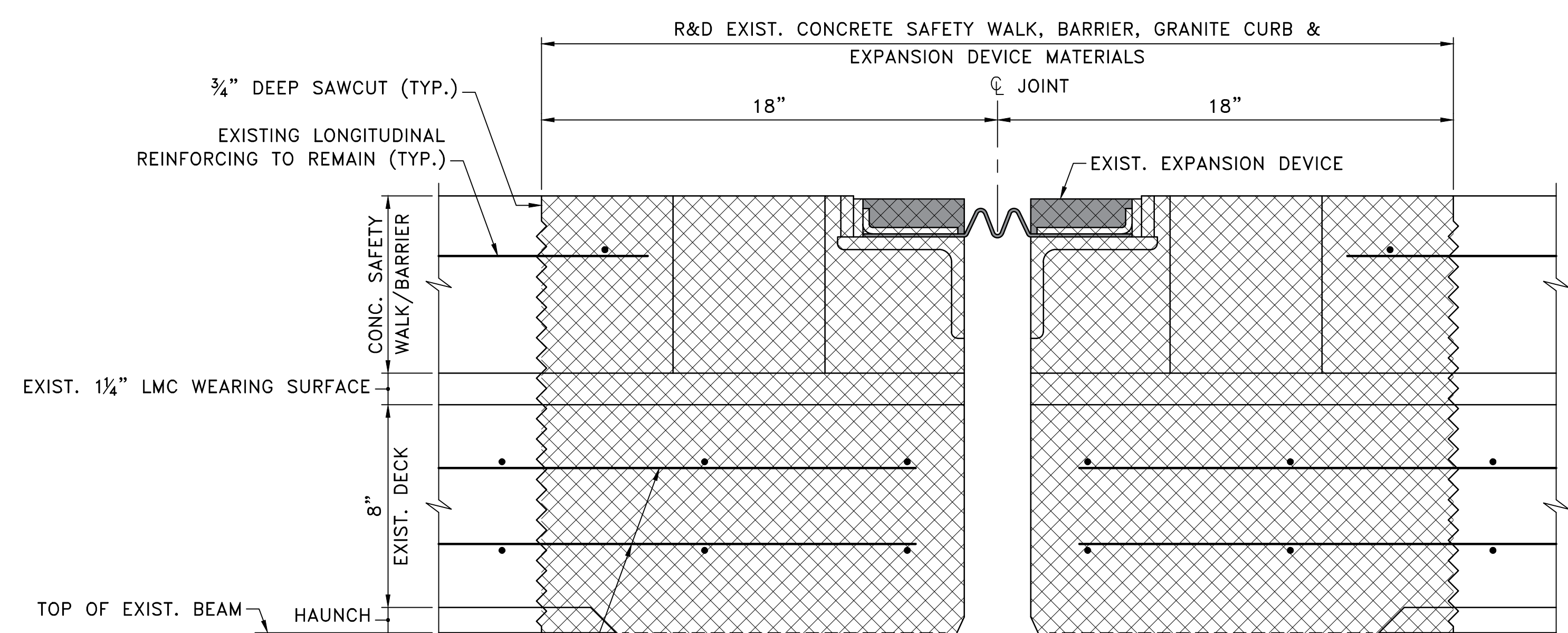
**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**JOINT DEMOLITION DETAILS - 1**



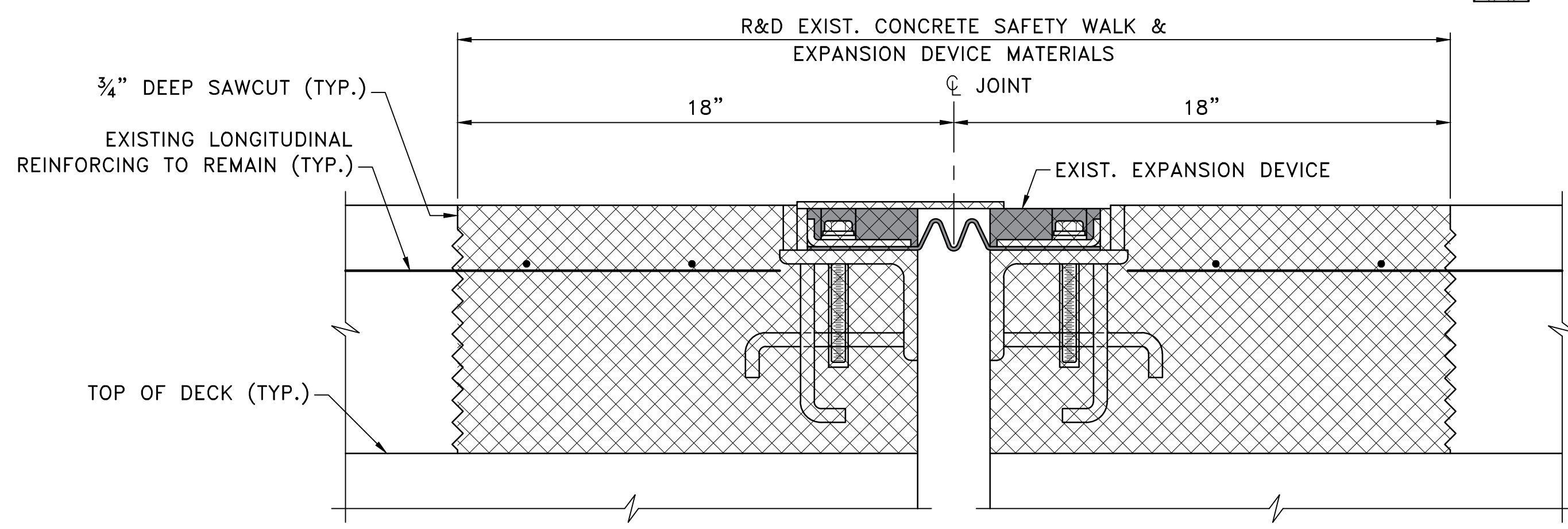
**NOTE:**  
ALL EXISTING REINFORCING WHICH EXTENDS FROM THE DECK AND THE TOPS OF THE END DIAPHRAGMS & BEAMS SHALL REMAIN.

**SECTION C**  
SCALE: 3" = 1'-0"

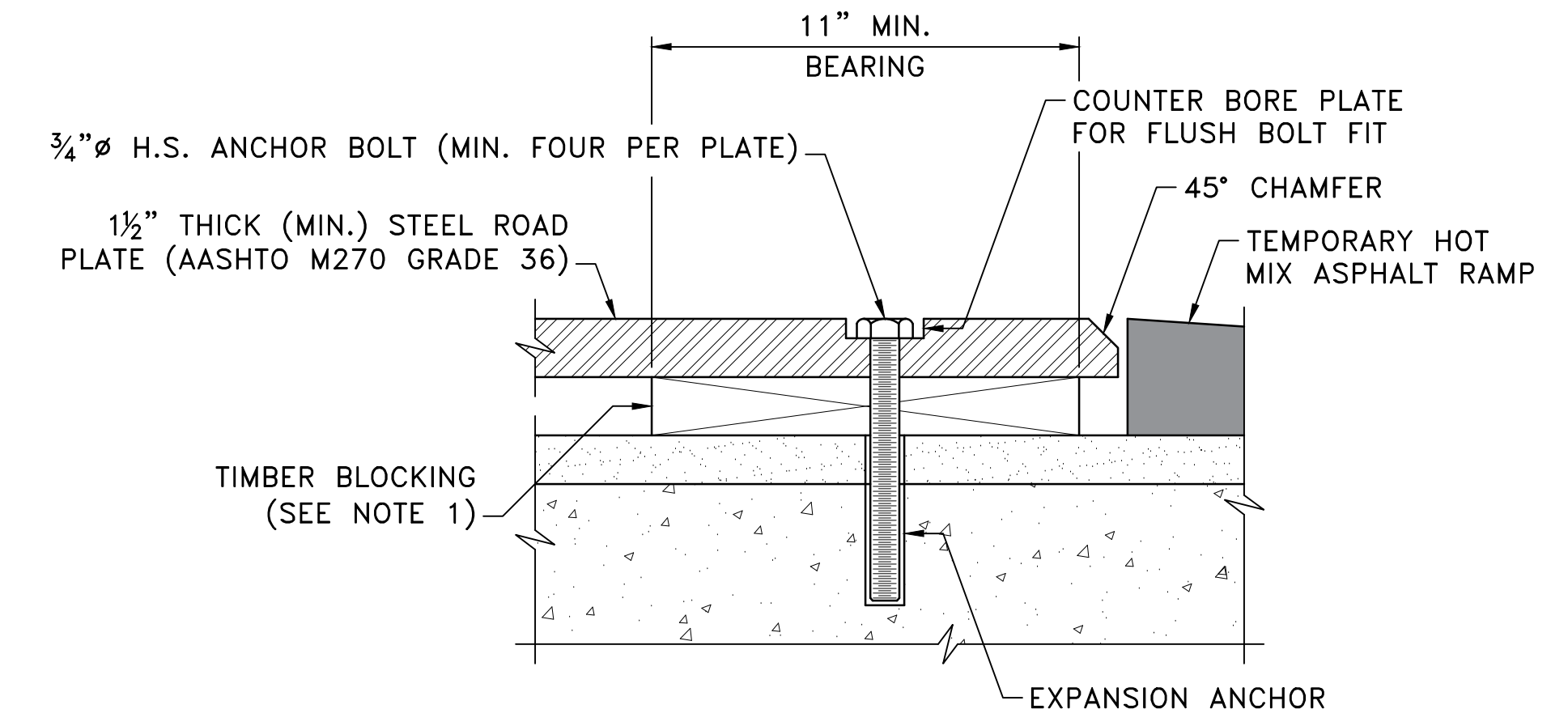
**LEGEND**  
DENOTES AREAS TO BE REMOVED AND DISPOSED



**SECTION E**  
SCALE: 3" = 1'-0"

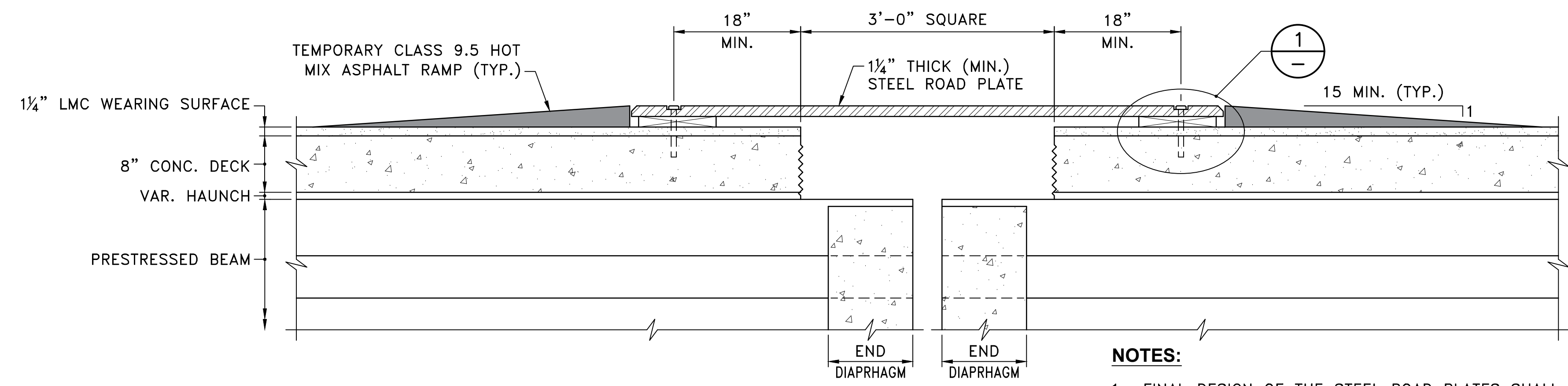


**SECTION D**  
SCALE: 3" = 1'-0"



- NOTES:**
1. PROVIDE CONTINUOUS BLOCKING AS REQUIRED TO PROTECT WORK BENEATH PLATE FROM TRAFFIC DEFLECTIONS.
  2. EXPANSION ANCHORS SHALL BE REPLACED AFTER REMOVING AND RE-INSTALLING ANCHOR BOLTS A MAXIMUM OF SEVEN (7) TIMES.
  3. UPON COMPLETION OF THE WORK, THE EXPANSION ANCHORS SHALL BE REMOVED AND THE HOLES FILLED WITH NON-SHRINK GROUT TO THE SATISFACTION OF THE ENGINEER.

**DETAIL 1**  
SCALE: 3" = 1'-0"



**STEEL ROAD PLATE DETAIL**  
SCALE: 1" = 1'-0"

- NOTES:**
1. FINAL DESIGN OF THE STEEL ROAD PLATES SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AS APPROVED BY THE ENGINEER.
  2. SECTION IS DRAWN LOOKING PARALLEL TO THE CENTERLINE OF BEARINGS. THE CONTRACTOR SHALL NOTE THAT THE CENTERLINE OF BEARINGS IS SKEWED IN RELATIONSHIP TO THE CENTERLINE OF THE ROADWAY AND SHALL TAKE THIS INTO ACCOUNT IN THE DESIGN/FABRICATION/INSTALLATION OF THE TRAFFIC PLATES. NO GAP SHALL BE ALLOWED AT THE CURBLINE.
  3. STEEL ROAD PLATES SHALL FOLLOW THE CROSS SLOPE OF THE ROADWAY.
  4. WIDTH OF THE PLATES SHALL BE DETERMINED BY THE CONTRACTOR BUT SHALL NOT BE LESS THAN 6 FEET.



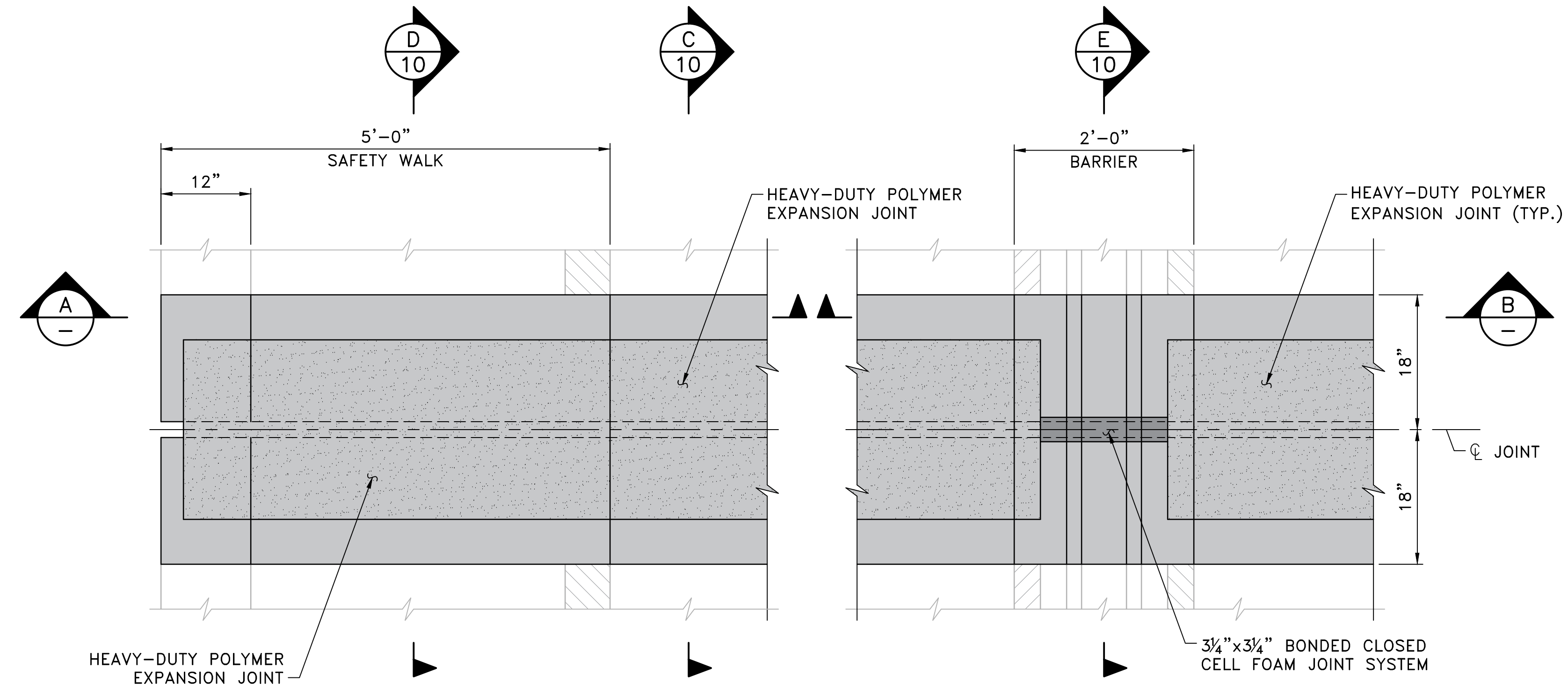
RHODE ISLAND  
TURNPIKE AND BRIDGE AUTHORITY

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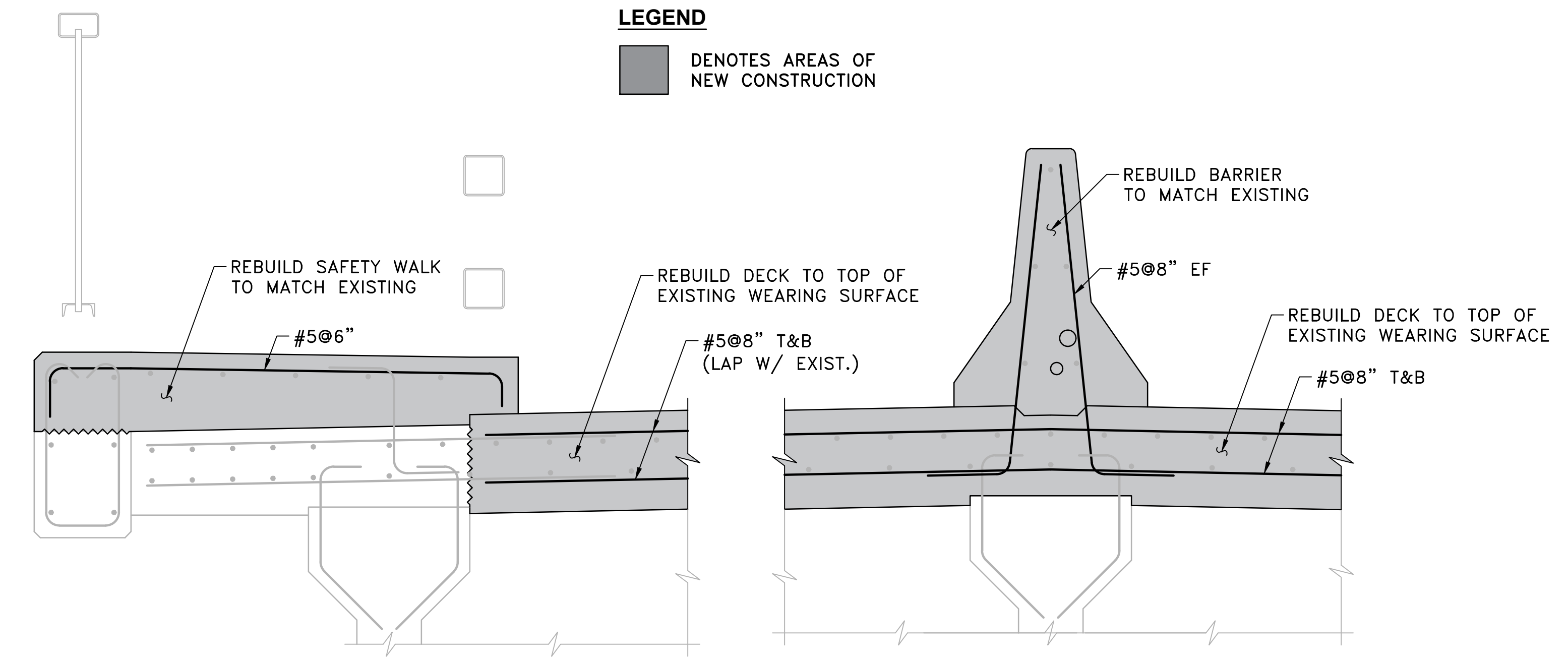
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**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**JOINT DEMOLITION DETAILS - 2**





**PROPOSED PLAN AT EXPANSION JOINT**  
SCALE: 1" = 1'-0"



**SECTION A**  
SCALE: 1" = 1'-0"

**SECTION B**  
SCALE: 1" = 1'-0"



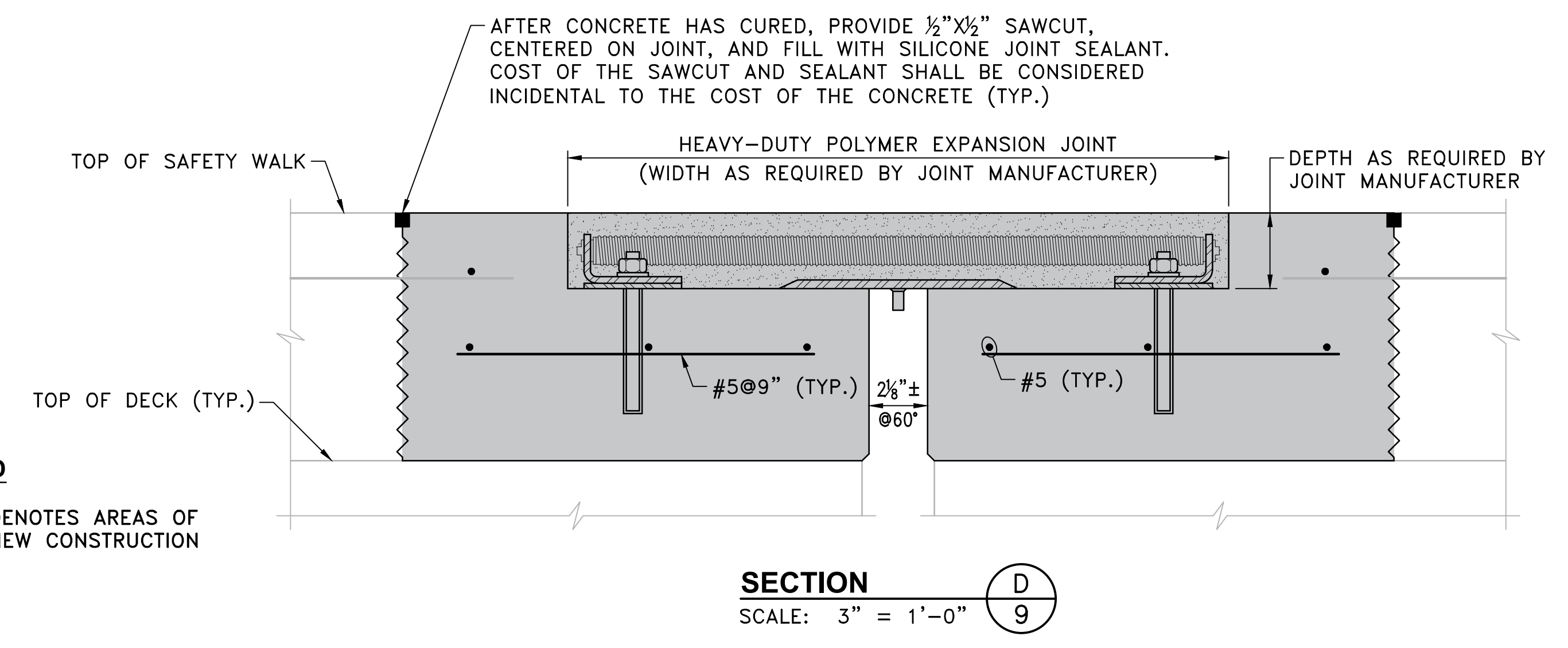
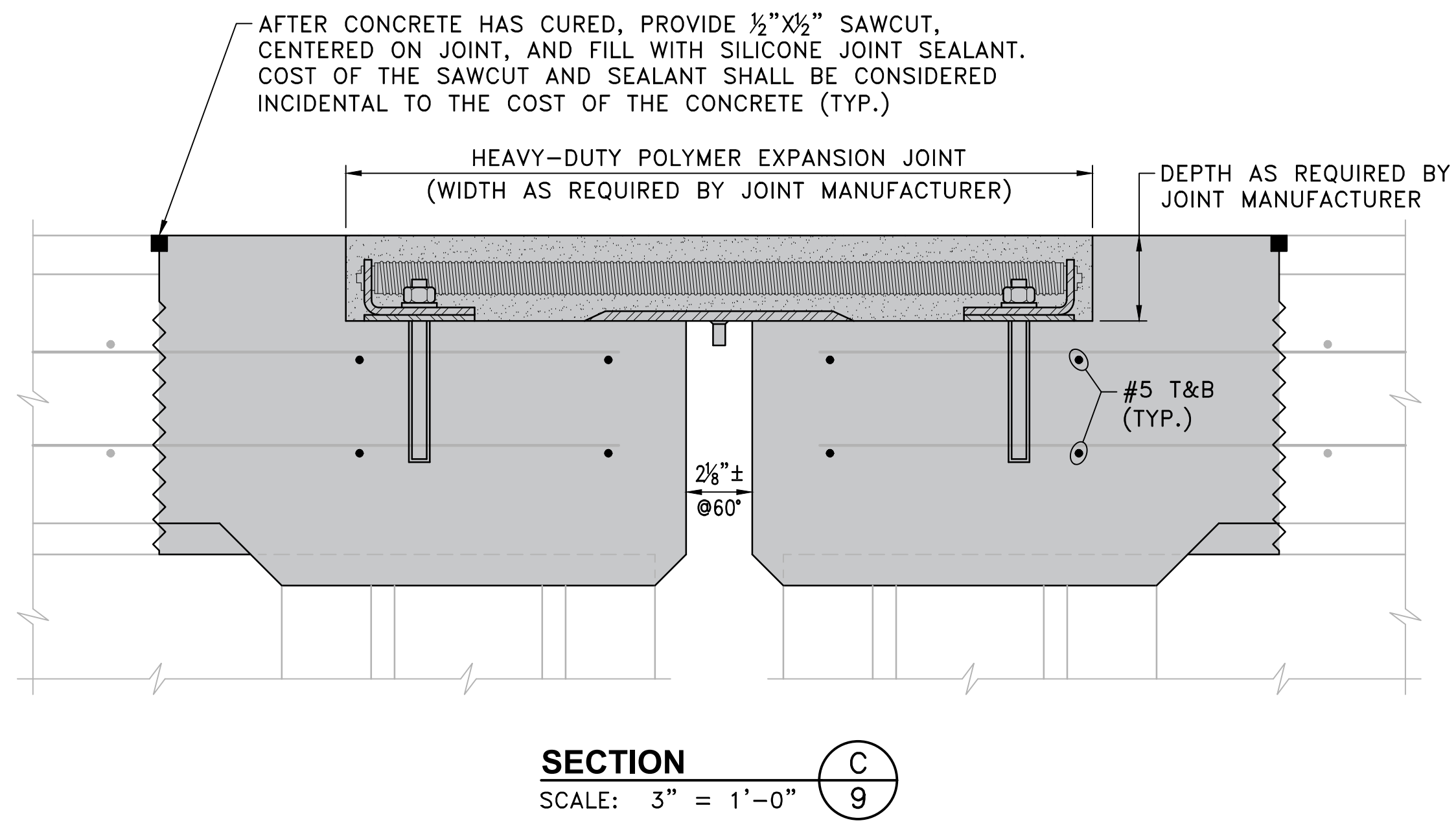
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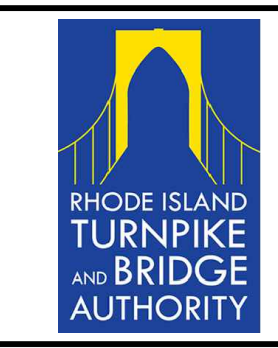
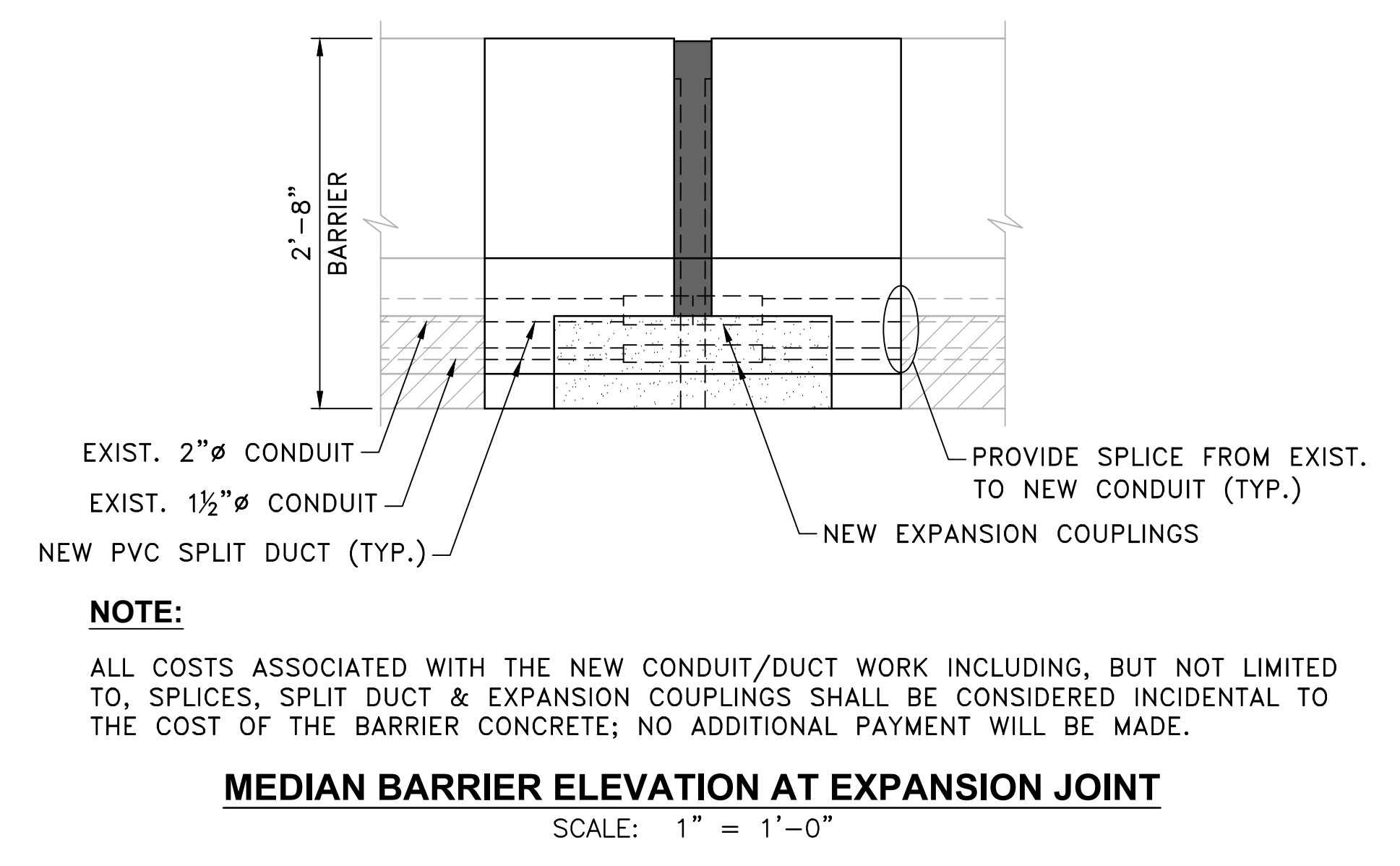
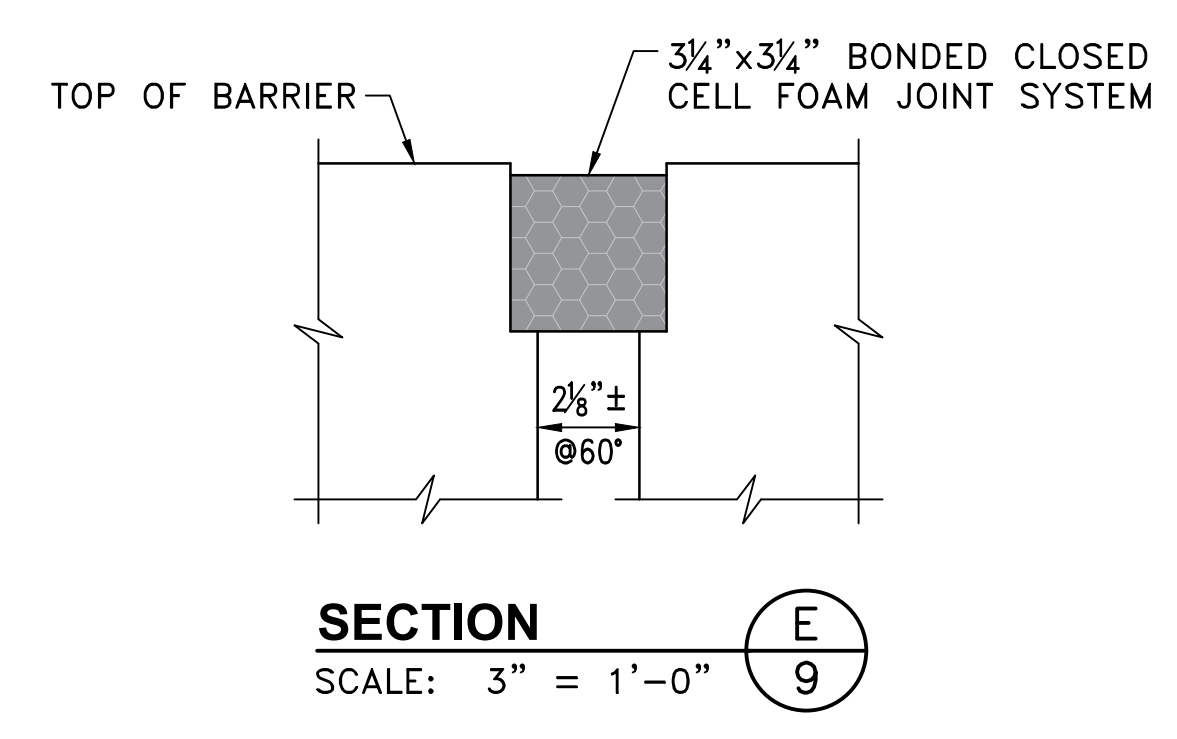
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**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
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NORTH KINGSTOWN RHODE ISLAND  
BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**JOINT RECONSTRUCTION DETAILS - 1**



**LEGEND**  
 DENOTES AREAS OF NEW CONSTRUCTION



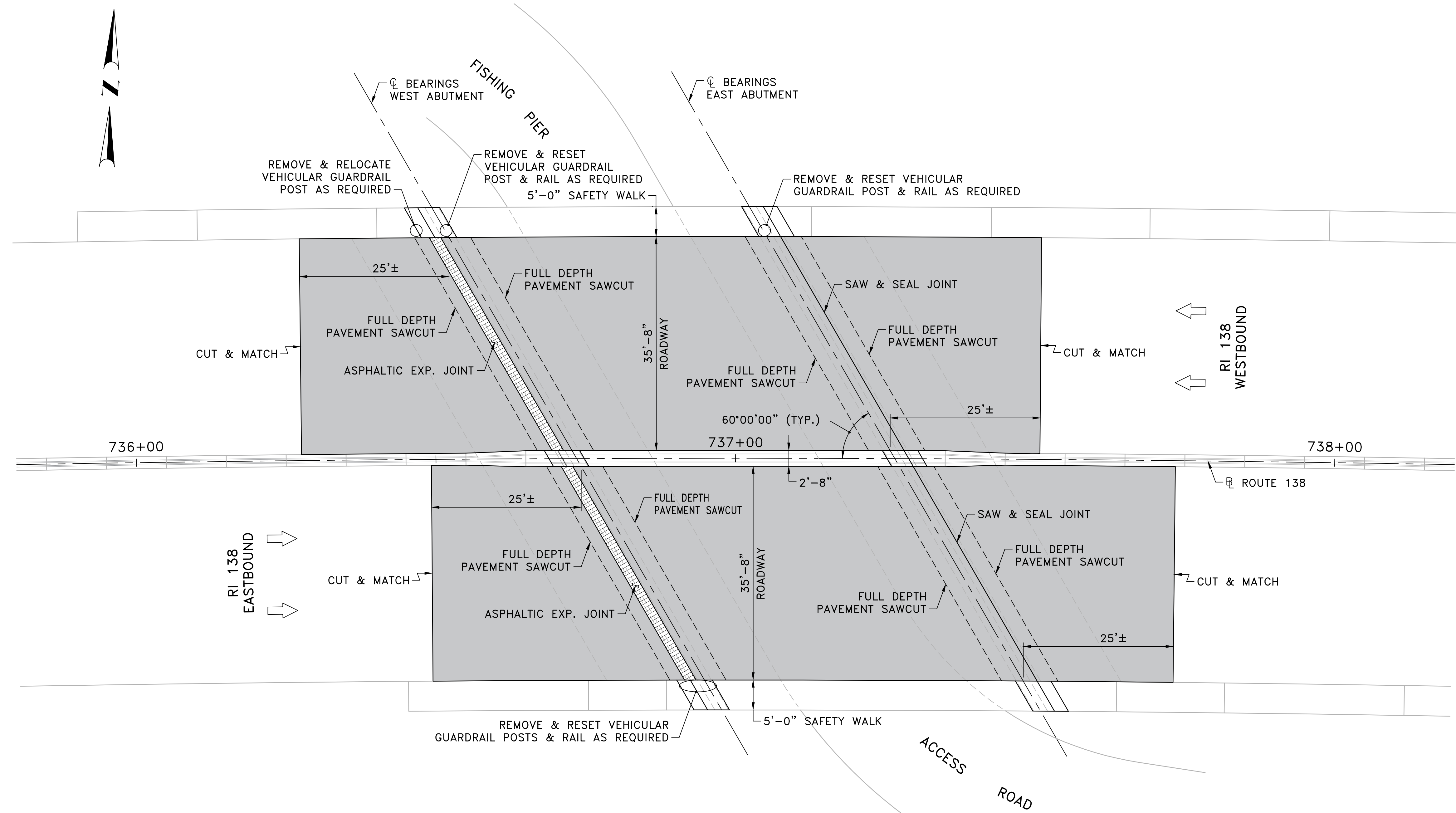
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**BRIDGE REPAIRS**  
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 BRIDGE NO. 080001 - TRESTLE STRUCTURE  
**JOINT RECONSTRUCTION DETAILS - 2**



**DESCRIPTION OF PROPOSED REPAIR WORK:**

- CONSTRUCTION OF DECK SLAB OVER BACKWALL DETAILS AT ALL ABUTMENTS.
- REPLACEMENT OF EXISTING JOINT AT THE WEST ABUTMENT WITH AN ASPHALTIC EXPANSION JOINT AND AT THE EAST ABUTMENT WITH SAW & SEAL JOINT.
- PARTIAL DEPTH REMOVAL OF EXISTING PAVEMENT AT APPROACHES AND BRIDGE AND REPAVE WITH HOT MIX ASPHALT.
- STEEL GIRDER REPAIRS AT LOCATIONS SHOWN IN THESE PLANS.
- CLEAN AND PAINT THE ENDS OF ALL BEAMS AT ABUTMENTS. PAINTING LIMITS TO INCLUDE 6'-0" OF BEAM, MEASURED FROM THE END OF THE BEAM, AND ALL STIFFENERS, CONNECTION PLATES, END DIAPHRAGMS, AND BEARINGS WITHIN THESE LIMITS. FINISH COLOR TO BE BROWN TO MATCH FEDERAL STANDARD 595A COLOR 10075.
- CLEAN AND PAINT ADDITIONAL AREAS OF THE BEAMS AS SPECIFIED IN THE DETAILS HEREIN.

**LEGEND:**

- DENOTES LIMITS OF ASPHALTIC EXPANSION JOINT
- DENOTES LIMITS OF HMA PAVEMENT



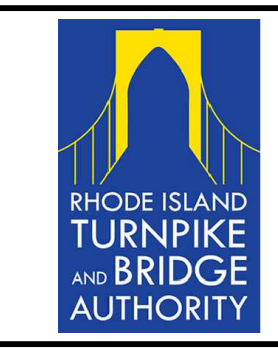
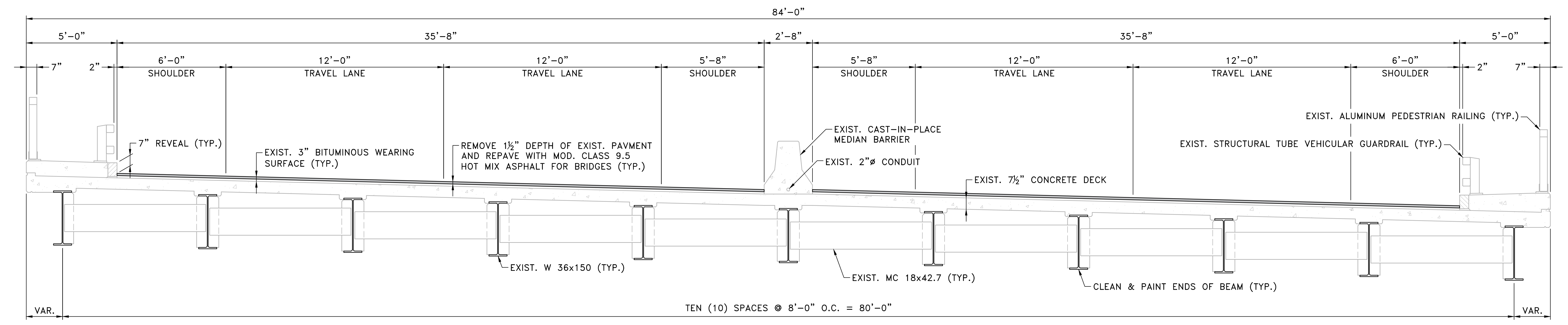
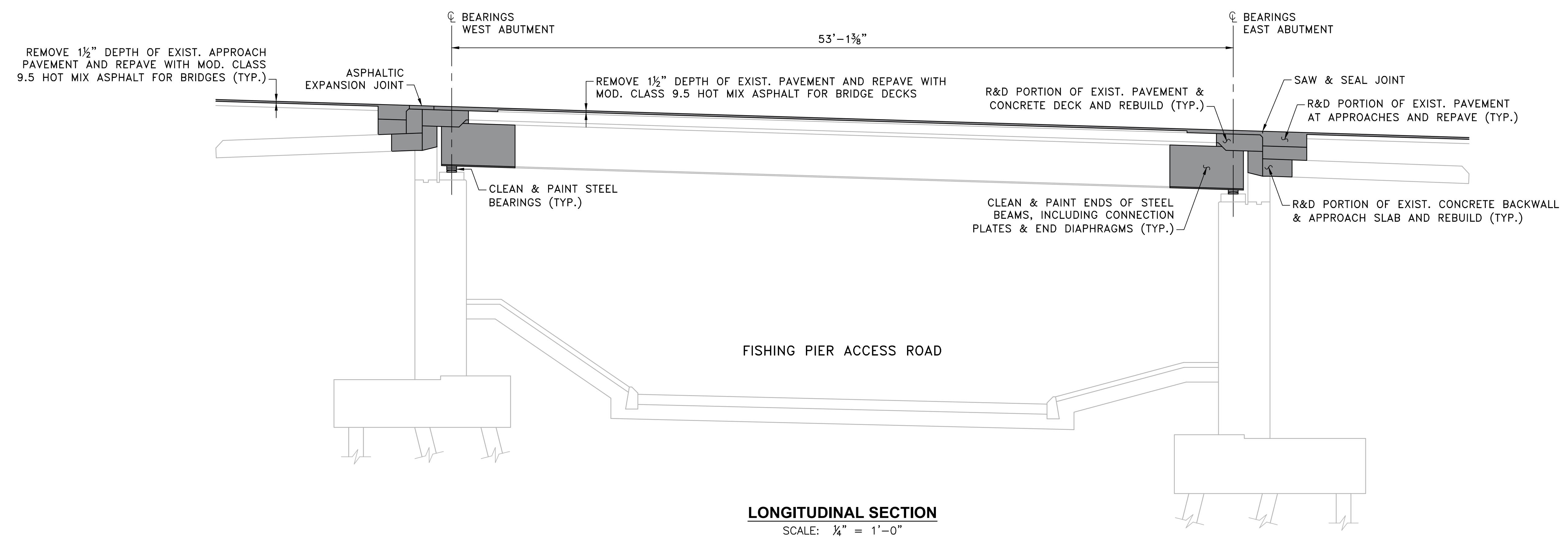
RHODE ISLAND  
TURNPIKE AND BRIDGE AUTHORITY

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SCALE: 1"=10'

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**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
**GENERAL PLAN**



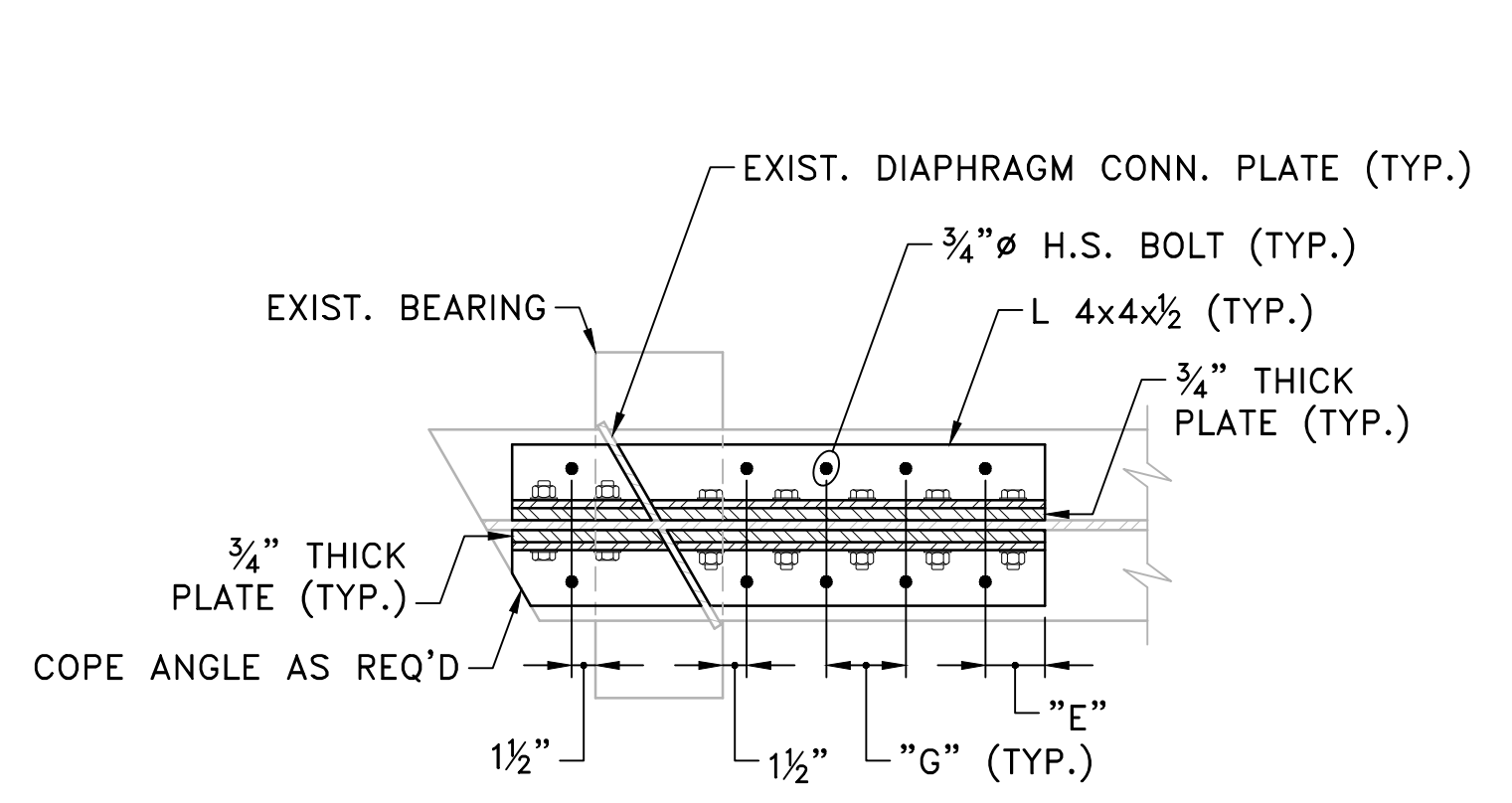
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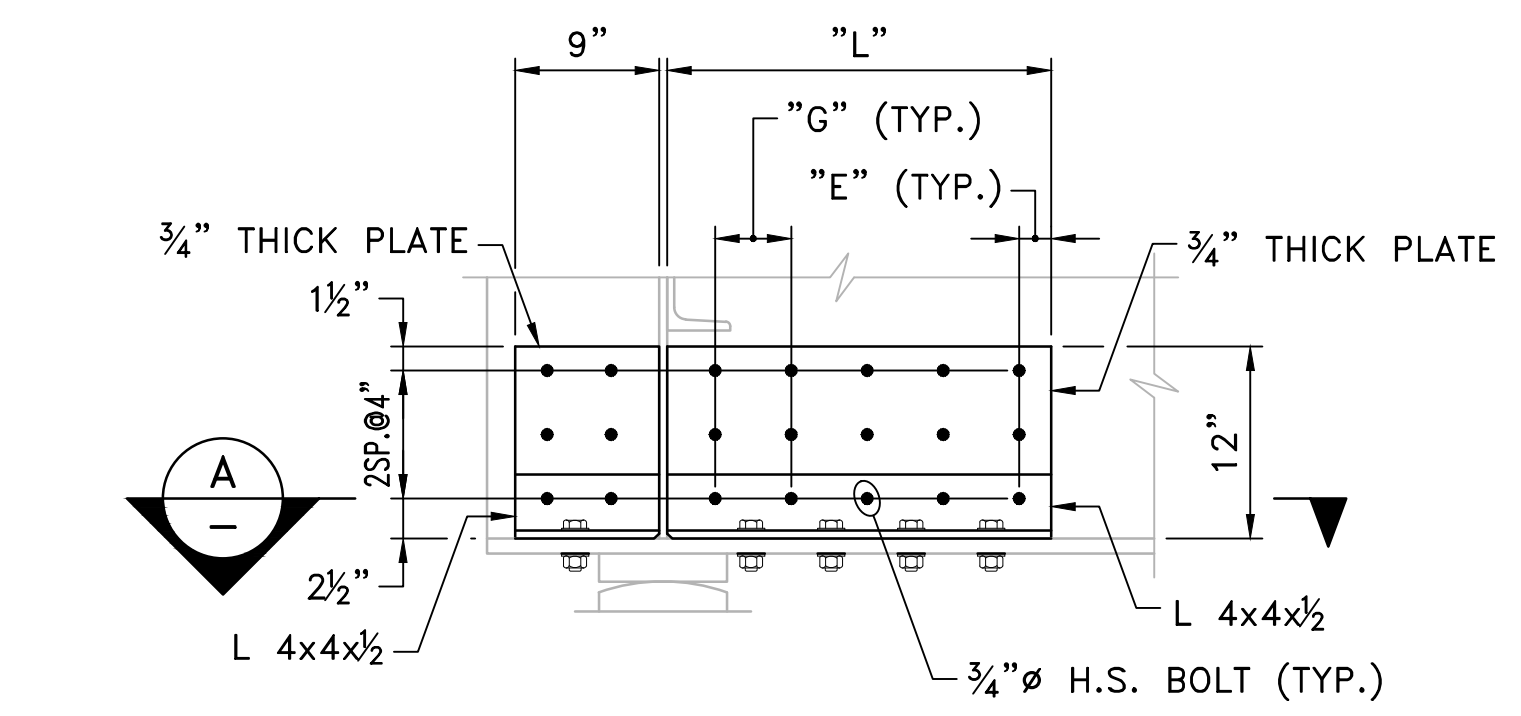
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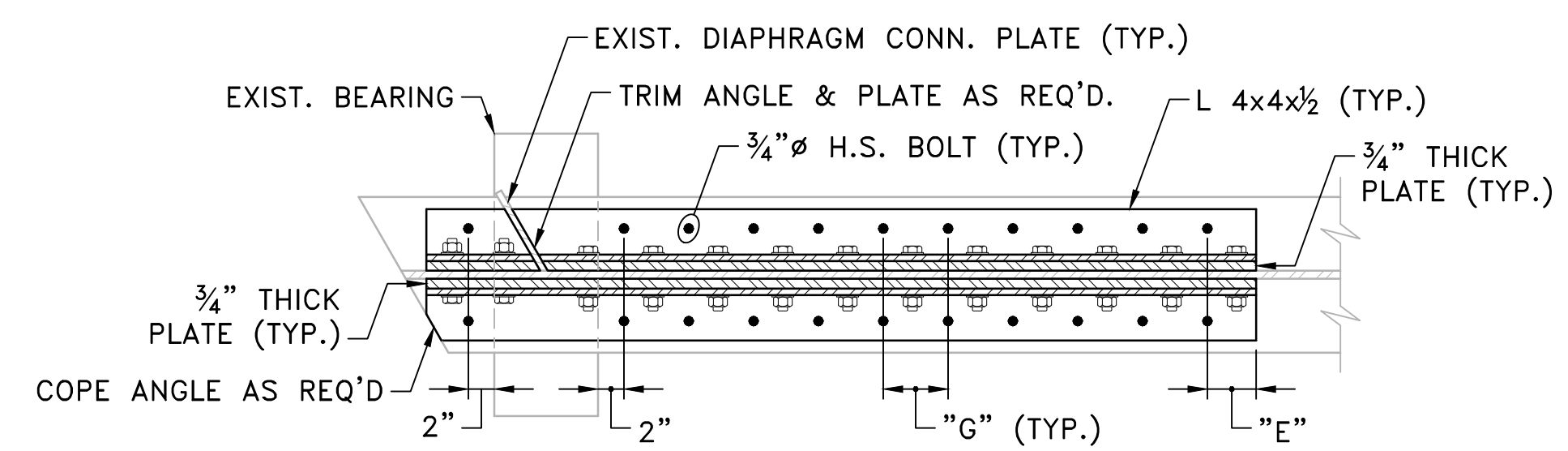
**BRIDGE REPAIRS**  
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FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
**TYPICAL SECTIONS**



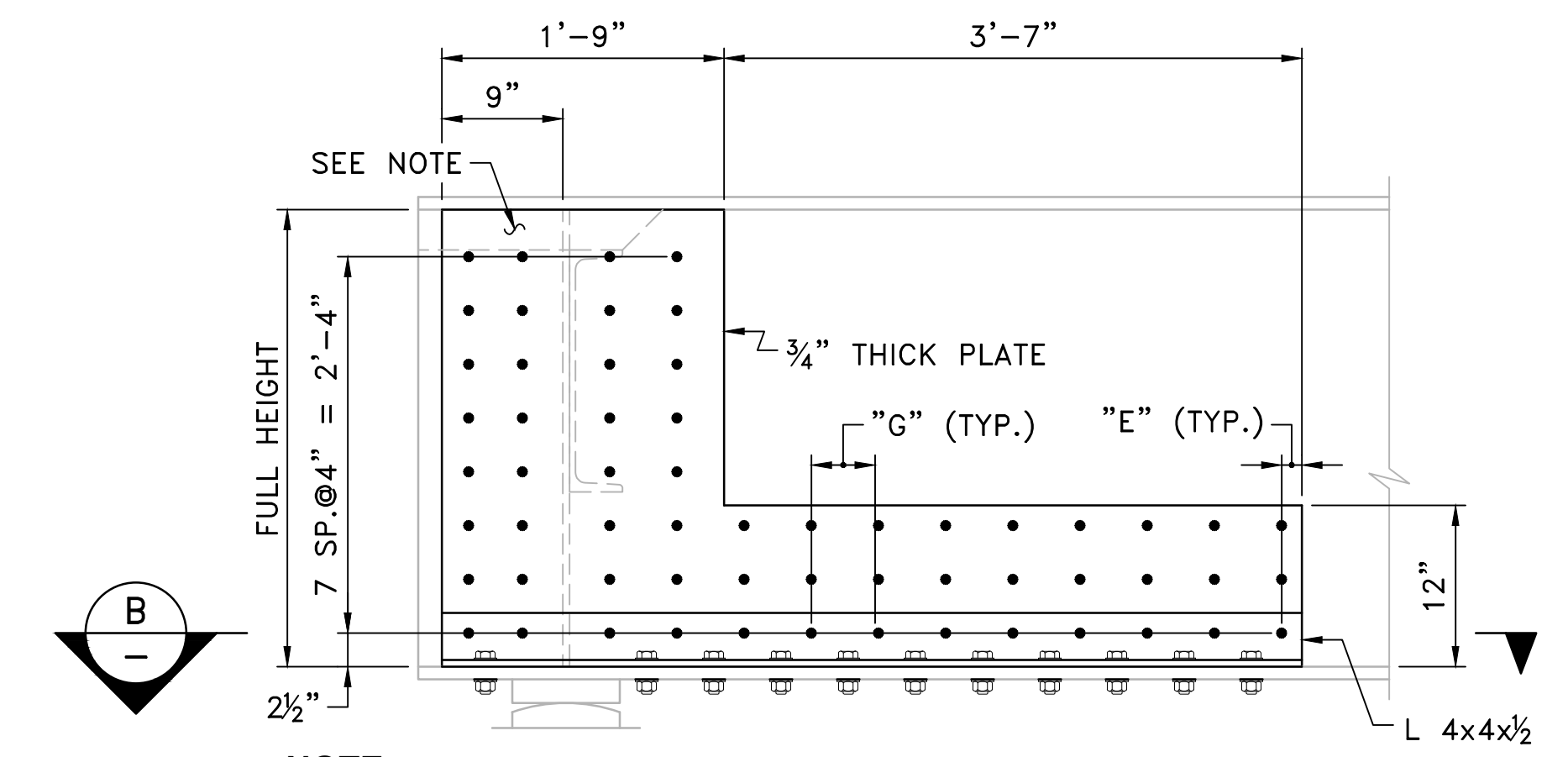
**SECTION A**  
SCALE: 1" = 1'-0"



**STEEL REPAIR DETAIL 1**  
SCALE: 1" = 1'-0"

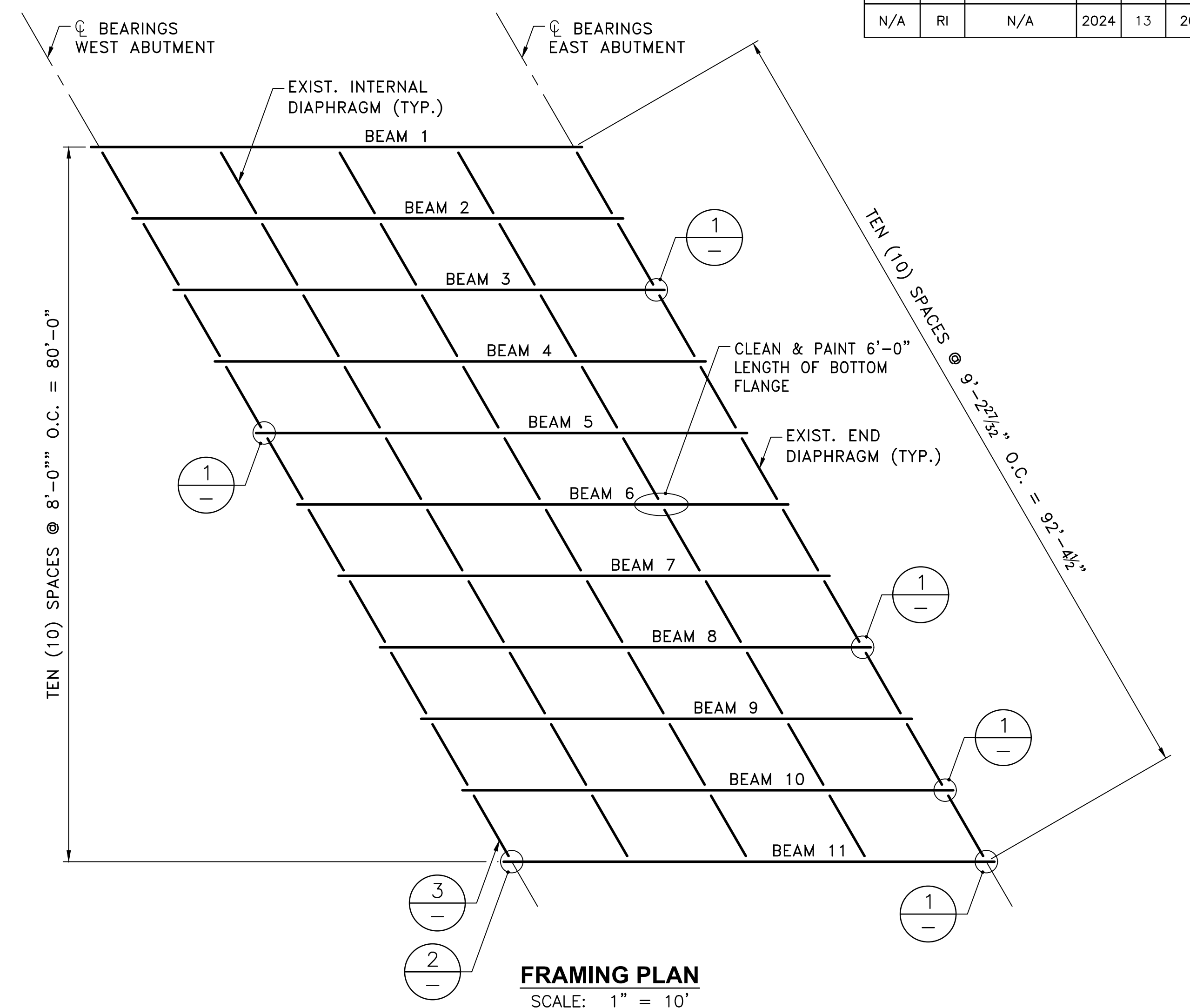


**SECTION B**  
SCALE: 1" = 1'-0"



**STEEL REPAIR DETAIL 2**  
SCALE: 1" = 1'-0"

**NOTE:**  
CONTRACTOR SHALL SCHEDULE THIS WORK SO THAT THE NEW DECK HAUNCH IS PLACED AFTER THE REPAIR PLATES ARE IN PLACE. REMOVAL OF PORTIONS OF THE NEW DECK HAUNCH WILL NOT BE ALLOWED.



**FRAMING PLAN**  
SCALE: 1" = 10'

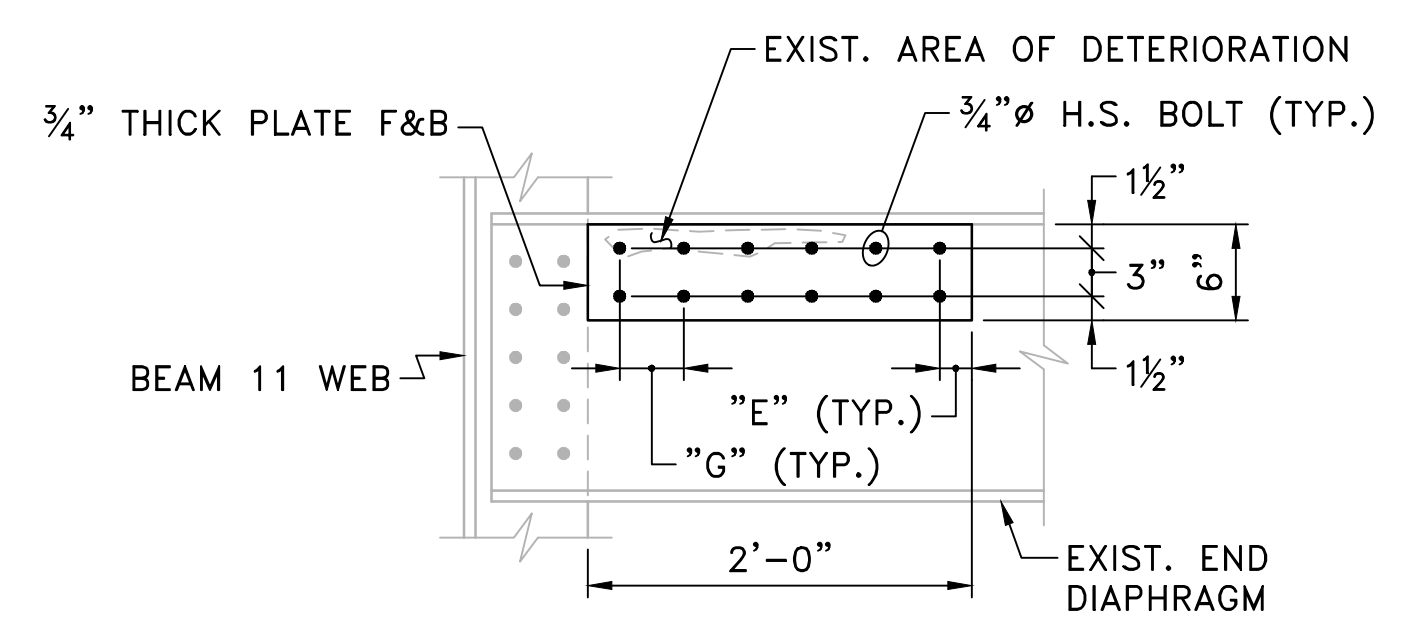
**STEEL REPAIR DETAIL 1 LENGTHS**

BEAM	LOCATION	"L"
3	EAST ABUT.	18"
5	WEST ABUT.	2'-0"
8	EAST ABUT.	2'-0"
10	EAST ABUT.	2'-6"
11	EAST ABUT.	5'-0"

**TYPICAL BOLT SPACINGS**

LOCATION	MIN.	MAX.
EDGE DISTANCE - "E"	1 1/2"	4"
GAUGE - "G"	3"	6"

UNLESS OTHERWISE NOTED, THE ABOVE BOLT SPACINGS SHALL BE UTILIZED IN THE FABRICATION OF THE STEEL REPAIR PLATES/MEMBERS.



**STEEL REPAIR DETAIL 3**  
SCALE: 1" = 1'-0"

**STEEL REPAIR NOTES**

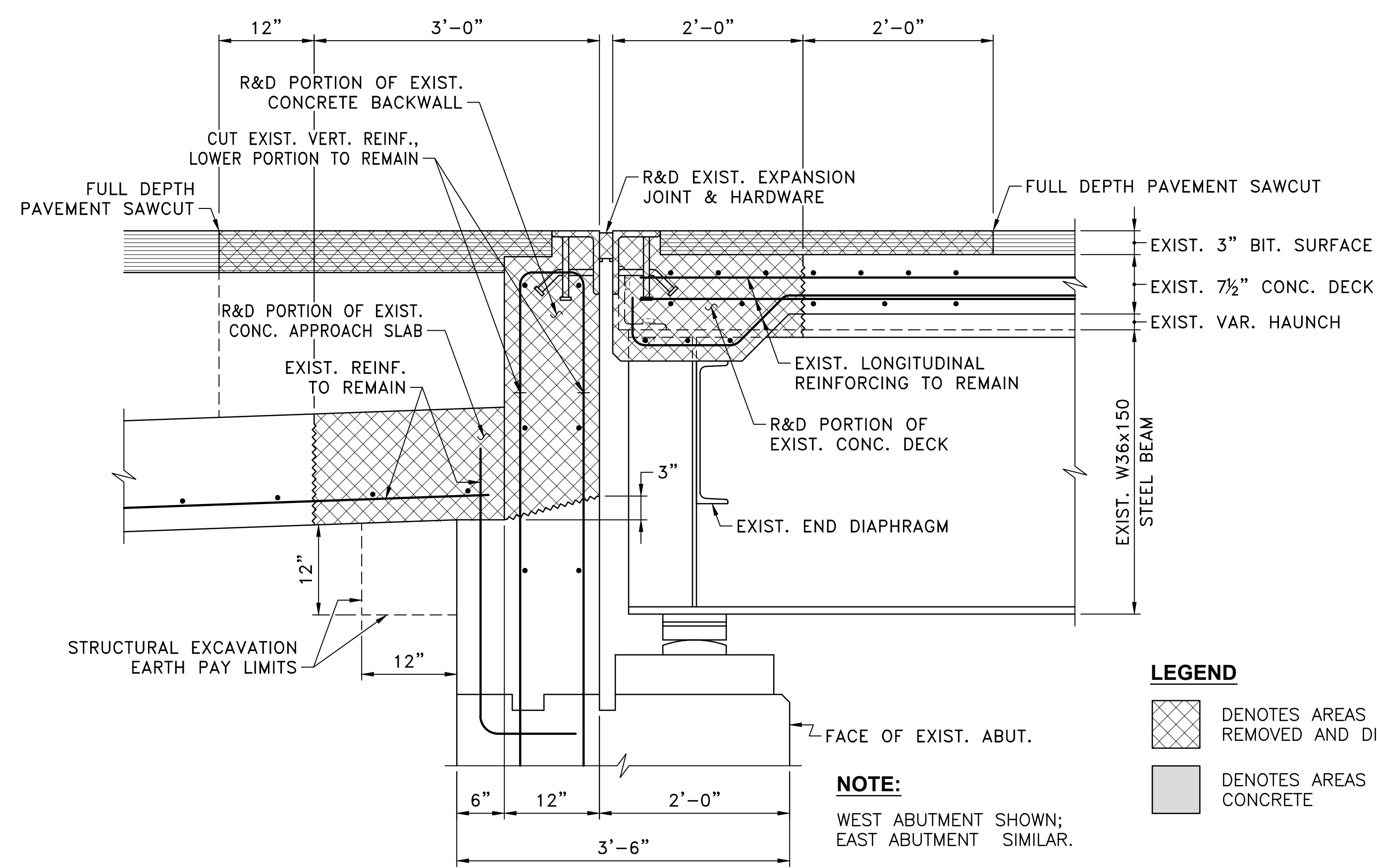
- EXISTING DETAILS AND DIMENSIONS SHOWN HAVE BEEN OBTAINED FROM AVAILABLE PLANS AND CURRENT INSPECTION REPORTS AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS AND DIMENSIONS. ANY DISCREPANCIES REQUIRING MODIFICATIONS TO THE PROPOSED DETAILS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER. THE CONTRACTOR SHALL RECORD ALL APPROPRIATE DIMENSIONS AND EXISTING LAYOUT INFORMATION TO ENSURE PROPER FIT-UP OF THE STEEL IN THE FIELD.
- PRIOR TO VERIFYING EXISTING CONDITIONS AND DIMENSIONS, THE CONTRACTOR SHALL CLEAN AND PRIME THE EXISTING STRUCTURAL STEEL TO THE SPECIFIED LIMITS. ANY CONDITIONS WARRANTING ADDITIONAL REPAIR LIMITS NOT SPECIFIED HEREIN SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
- AFTER VERIFICATION OF EXISTING CONDITIONS AND DIMENSIONS, THE CONTRACTOR SHALL PREPARE AND SUBMIT DETAILED SHOP DRAWINGS FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- SHIELDING WILL NOT BE REQUIRED FOR THIS WORK. THE CONTRACTOR, THOUGH, SHALL ENSURE THAT NO DEBRIS FALLS TO THE ROADWAY BELOW.
- THE CONTRACTOR IS RESPONSIBLE FOR THE OVERALL FIT OF THE BOLTED CONNECTIONS. FINAL BOLT SPACING AND DETAILS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE INCLUDED ON THE SHOP DRAWINGS.
- REMNANTS OF EXISTING WELDS AND PLATES SHALL BE GROUND SMOOTH AS REQUIRED TO INSTALL PROPOSED REPAIR SHAPES AND PLATES.
- PROPOSED REPAIR PLATES SHALL BE COPE, AS REQUIRED, AT EXISTING WELDS THAT ARE TO REMAIN.
- ANY BOLTS THAT PASS THROUGH AN EXISTING HOLE CAUSED BY THE DETERIORATION OF THE STEEL SHALL BE PROVIDED WITH A PLATE WASHER/FILLER PLATE OF A THICKNESS EQUAL TO THE ORIGINAL THICKNESS OF THE EXISTING MATERIAL. THIS WASHER/FILLER PLATE SHALL BE PLACED WITHIN THE HOLE BETWEEN THE PROPOSED PLATES. IF THE HOLE IS SMALLER THAN TWICE THE DIAMETER OF THE BOLT, THE PLATE WASHER/FILLER PLATE MAY BE OMITTED.
- EXISTING STEEL SURFACE SHALL BE CLEANED AND PRIMED IN ACCORDANCE WITH ITEM CODE 825.8040 PRIOR TO INSTALLING THE PROPOSED STEEL. AN EPOXY PASTE ADHESIVE SHALL THEN BE APPLIED TO THE EXISTING STEEL, SCREED INTO POSITION TO THE APPROXIMATE ORIGINAL SURFACE PROFILE OF THE STEEL. A SLIGHT EXCESS SHALL BE PROVIDED THAT CAN BE SQUEEZED OUT WHEN THE PROPOSED STEEL PLATES ARE INSTALLED. THE PROPOSED PLATES SHALL BE INSTALLED WHILE THE EPOXY IS STILL PLASTIC. ALL BOLTS SHALL BE INSTALLED "SNUG-TIGHT" IN ORDER TO BRING THE STEEL PARTS INTO THE POSITION SHOWN IN THE DETAILS AND TO SQUEEZE OUT THE EXCESS EPOXY; FINAL TIGHTENING OF THE BOLTS SHALL BE DONE AFTER THE EPOXY HAS CURED. EXCESS EPOXY SHALL BE CLEANED AWAY PRIOR TO PAINTING.
- THE EPOXY PASTE ADHESIVE SHALL HAVE HIGH STRENGTH, NON-SAG, MOISTURE-TOLERANT PROPERTIES AND SHALL BE INCLUDED ON THE RIDOT APPROVED MATERIALS LIST. THE MATERIAL SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.
- THE REQUIRED REMOVAL & DISPOSAL OF ANY EXISTING STEEL OR CONCRETE SHALL BE IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE STEEL REPAIR.
- UNLESS OTHERWISE NOTED, ALL WORK AND MATERIALS DESCRIBED WITHIN THESE NOTES SHALL BE INCLUDED IN THE COST OF THE STEEL REPAIR.
- REPAIR PLATE SIZES AND REPAIR TYPE LOCATIONS SHOWN ARE THE MINIMUM REQUIRED REPAIRS. THE CONTRACTOR SHALL NOTE THAT THE SIZES OF THE PLATES MAY BE INCREASED AND ADDITIONAL REPAIR LOCATIONS MAY BE ADDED, AS REQUIRED BY THE ENGINEER. CONTRACT UNIT PRICES SHALL BE USED FOR THE PAYMENT OF THE INCREASED PLATE SIZES AND THE ADDED LOCATIONS, PROVIDED THAT THE ADDED LOCATIONS ARE ALONG THE SAME BEARING LINE AS A LOCATION NOTED IN THESE PLANS. NO OTHER ADDITIONAL PAYMENT SHALL BE MADE FOR THESE INCREASES/ADDITIONS, IN ACCORDANCE WITH SUBSECTION 109.03 OF THE RI STANDARD SPECIFICATIONS.



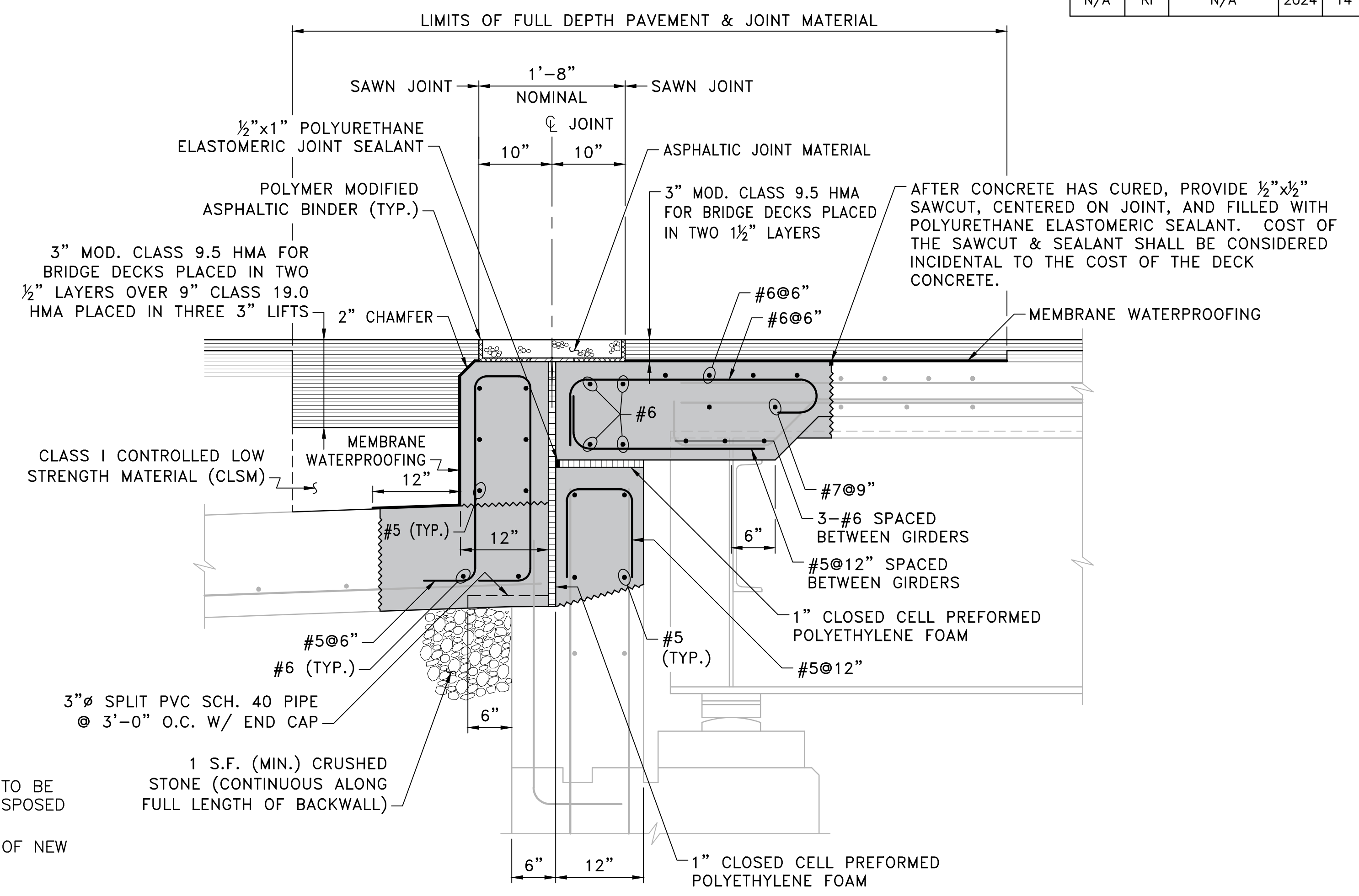
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**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
**STEEL REPAIR DETAILS**



**EXISTING ROADWAY SECTION AT ABUTMENT**  
SCALE: 1" = 1'-0"



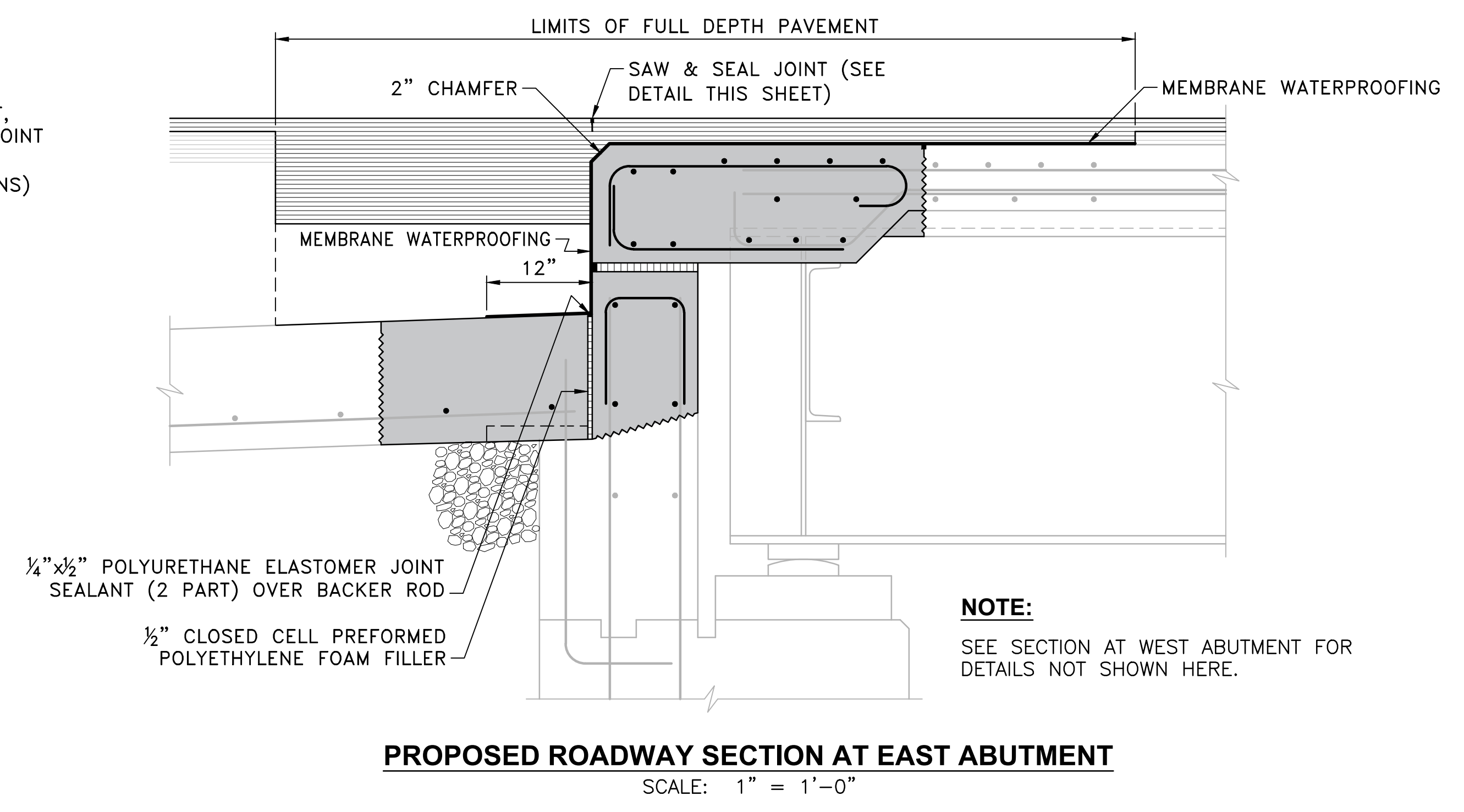
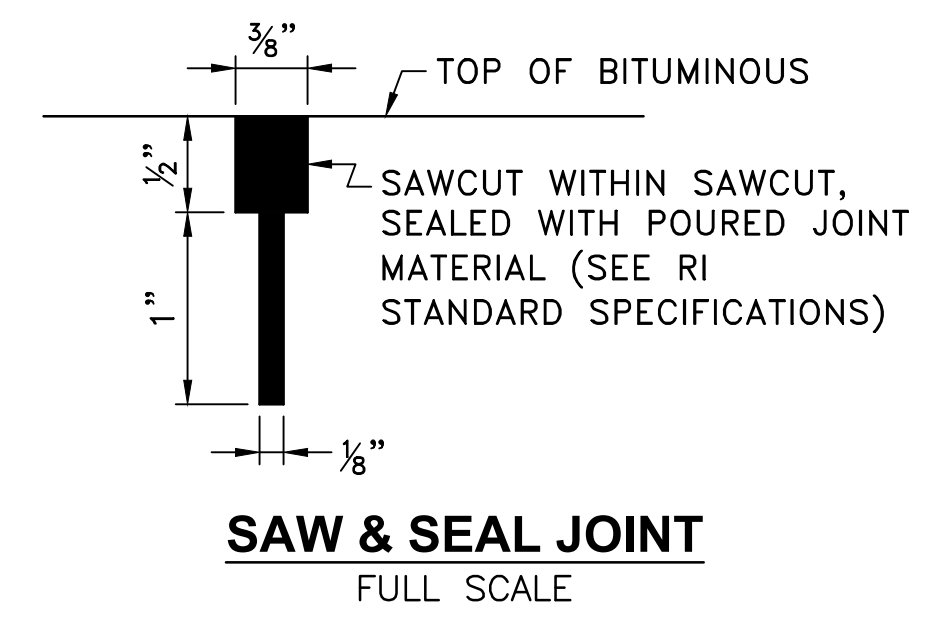
**PROPOSED ROADWAY SECTION AT WEST ABUTMENT**  
SCALE: 1" = 1'-0"

**NOTES:**

1. THE COST OF FURNISHING AND INSTALLING THE CRUSHED STONE AND PVC SPLIT PIPE DRAINS AT THE ABUTMENTS JOINTS SHALL BE CONSIDERED INCIDENTAL TO, AND INCLUDED WITHIN THE PAYMENT FOR, THE CONCRETE REQUIRED FOR THE REBUILDING OF THE APPROACH SLAB AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.
2. ANY REQUIRED BACKFILLING AND COMPACTION SHALL BE CONSIDERED INCIDENTAL; NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS WORK.
3. PROPOSED TEMPORARY SHORING AND BRACING SHALL BE INSTALLED TO PREVENT UNDERMINING OF THE EXISTING APPROACH ROADWAY. THE SHORING AND BRACING SHALL BE REMOVED AND DISPOSED UPON COMPLETION OF THE WORK. THE COST OF THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EXCAVATION; NO ADDITIONAL PAYMENT SHALL BE MADE FOR THIS WORK.
4. PAY LIMITS FOR ASPHALTIC EXPANSION JOINT SYSTEM IS FROM FACE OF CURB TO FACE OF CURB.

**SEQUENCE FOR BRIDGE JOINT WORK AND REPAVING:**

1. REMOVE AND DISPOSE MATERIALS AS CALLED FOR IN THE DETAILS.
2. RECONSTRUCT APPROACH SLAB, WITH NEW HAUNCH, BACKWALL, WINGWALL, DECK OVER BACKWALL, AND BARRIERS, AS CALLED FOR IN THE DETAILS.
3. INSTALL NEW WATERPROOFING MEMBRANE AND REPAVE (FULL DEPTH).
4. AFTER FULL WIDTH OF BRIDGE JOINT WORK HAS BEEN COMPLETED, REMOVE 1 1/2" DEPTH OF PAVEMENT ON THE BRIDGE AND APPROACHES, CLEAN & SWEEP THE SURFACE, APPLY TACK COAT, AND REPAVE WITH 1 1/2" DEPTH OF MOD. CLASS 9.5 HOT MIX ASPHALT FOR BRIDGE DECKS.
5. AT EAST ABUTMENT, SAWCUT & SEAL JOINT.
6. AT WEST ABUTMENT, SAWCUT & REMOVE 1'-8" (NOMINAL) WIDTH OF PAVEMENT AND INSTALL NEW ASPHALTIC JOINT MATERIALS.



**PROPOSED ROADWAY SECTION AT EAST ABUTMENT**  
SCALE: 1" = 1'-0"

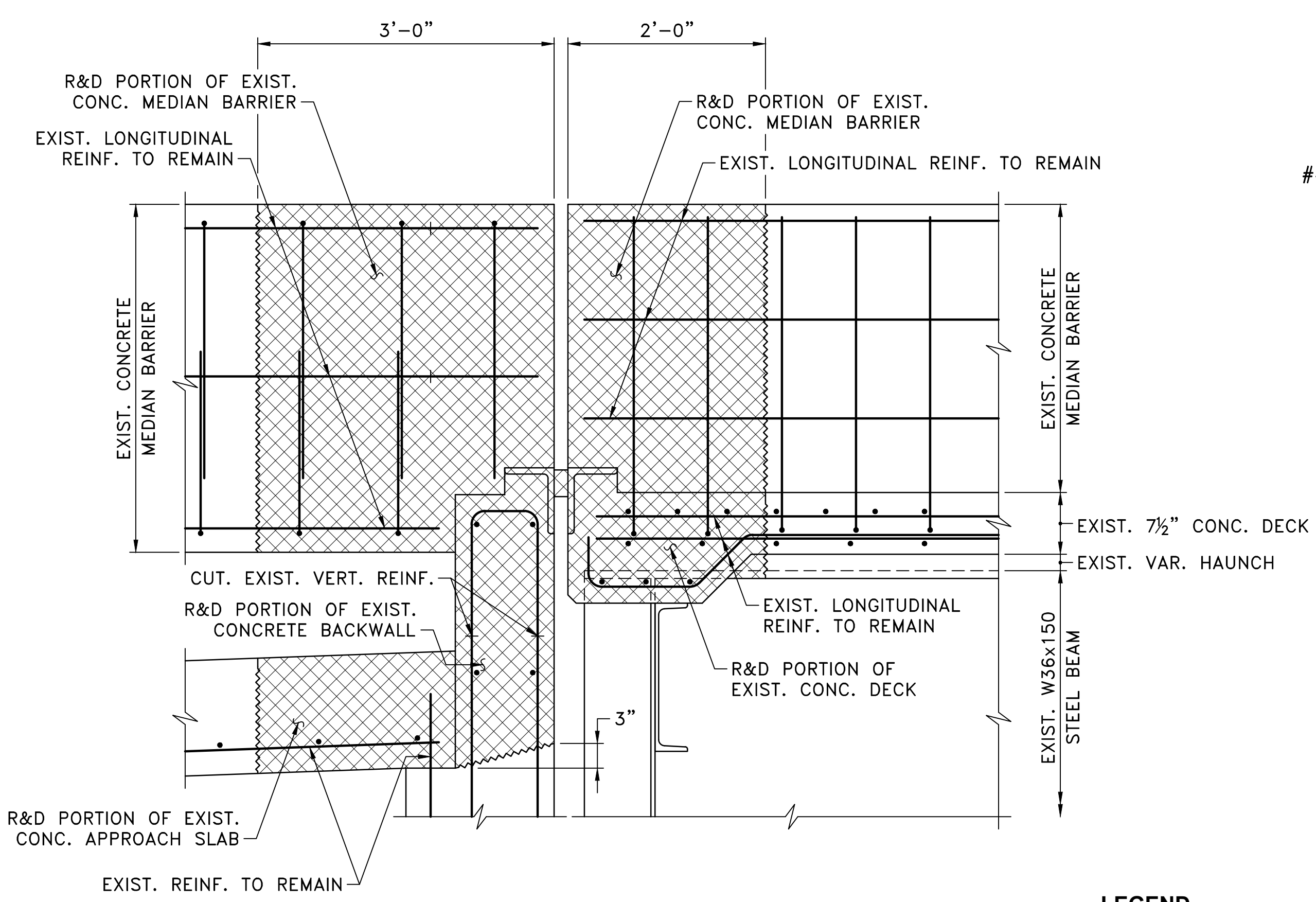


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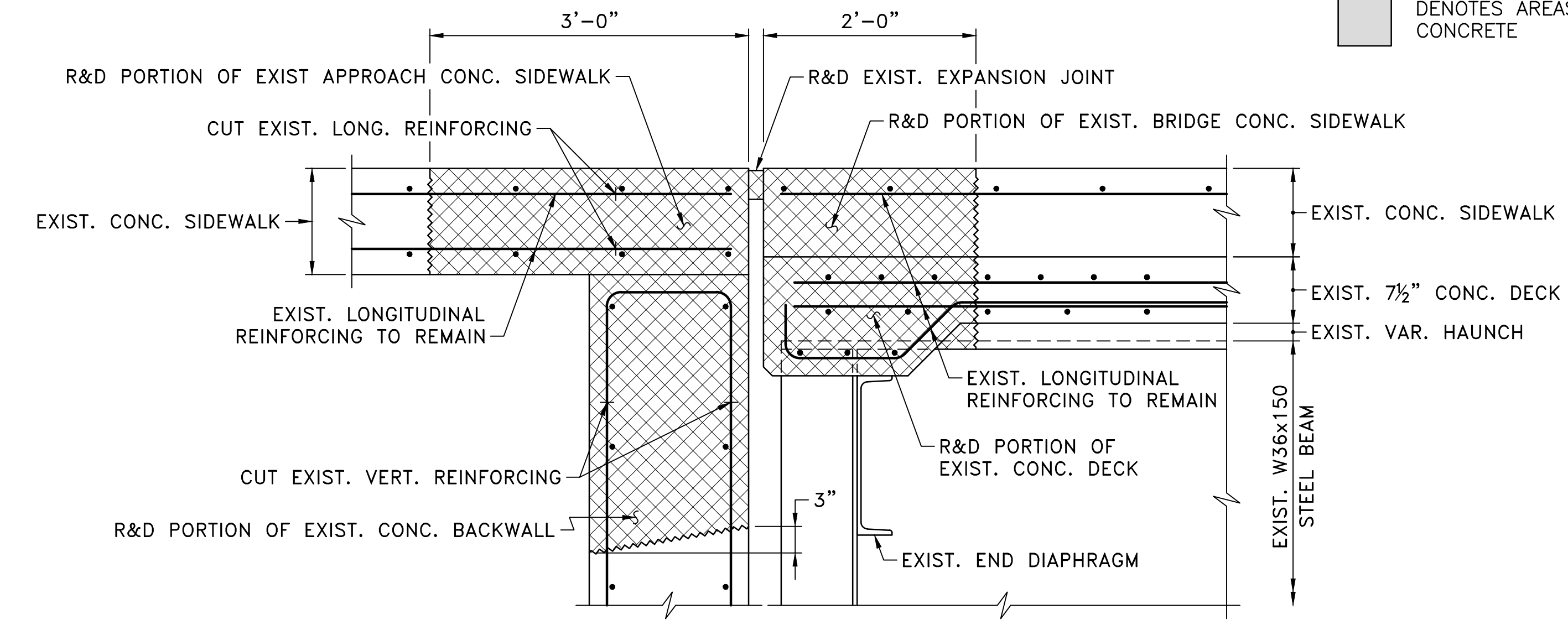
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**JOINT DETAILS - 1**



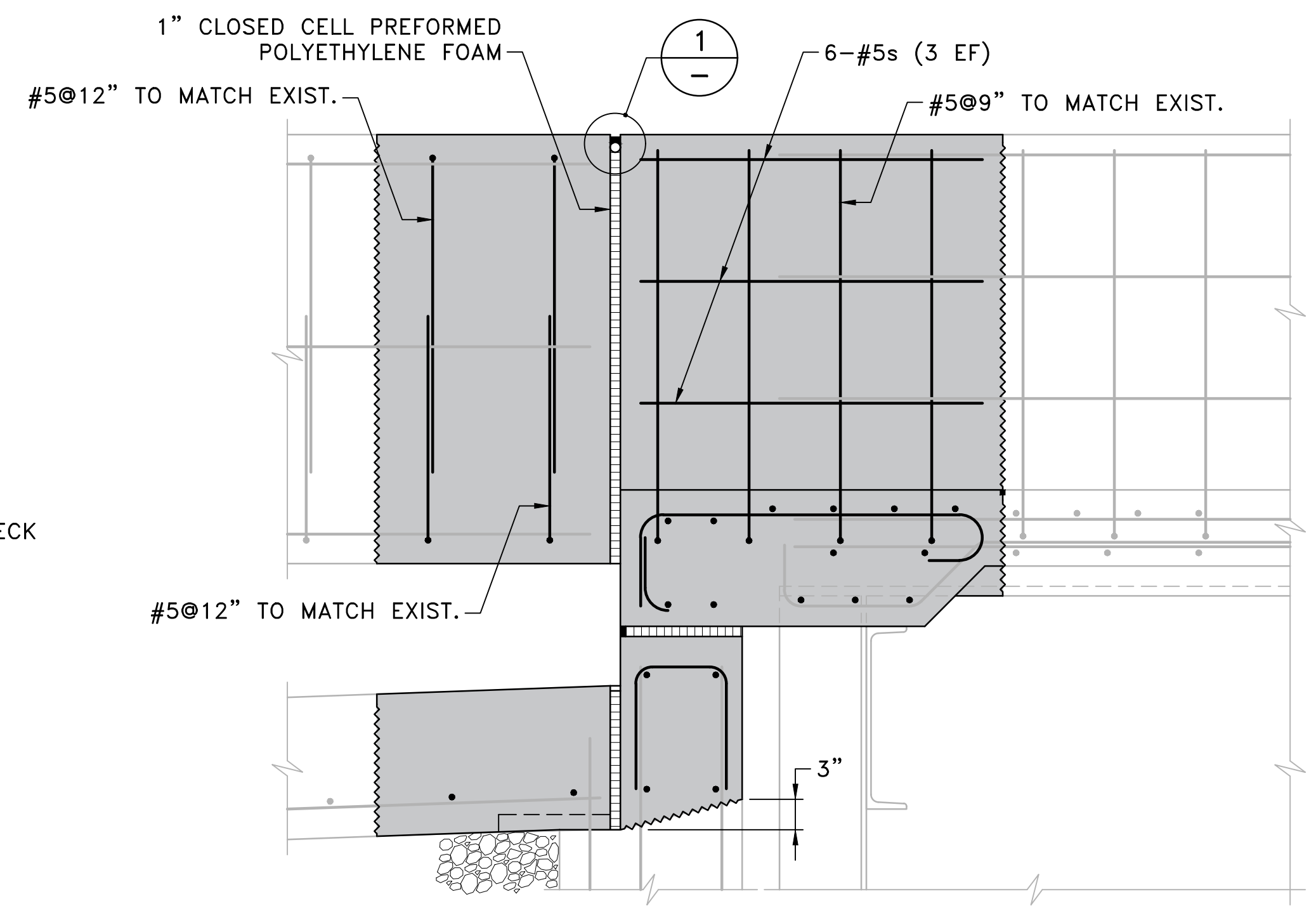
**EXISTING BARRIER SECTION AT ABUTMENT**  
SCALE: 1" = 1'-0"

**LEGEND**

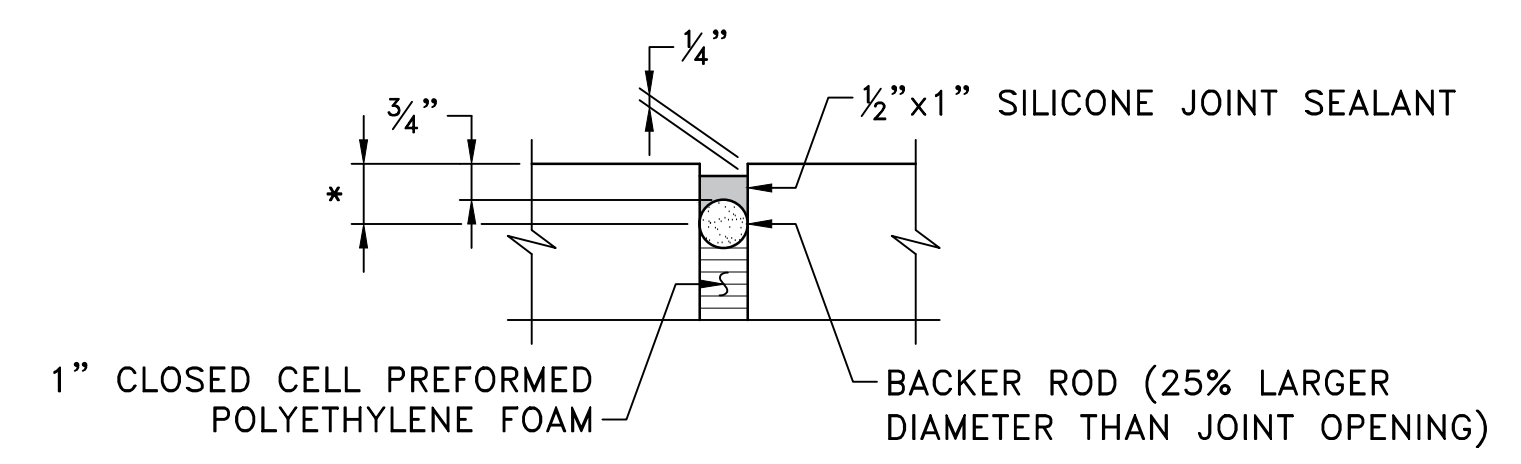
- DENOTES AREAS TO BE REMOVED AND DISPOSED
- DENOTES AREAS OF NEW CONCRETE



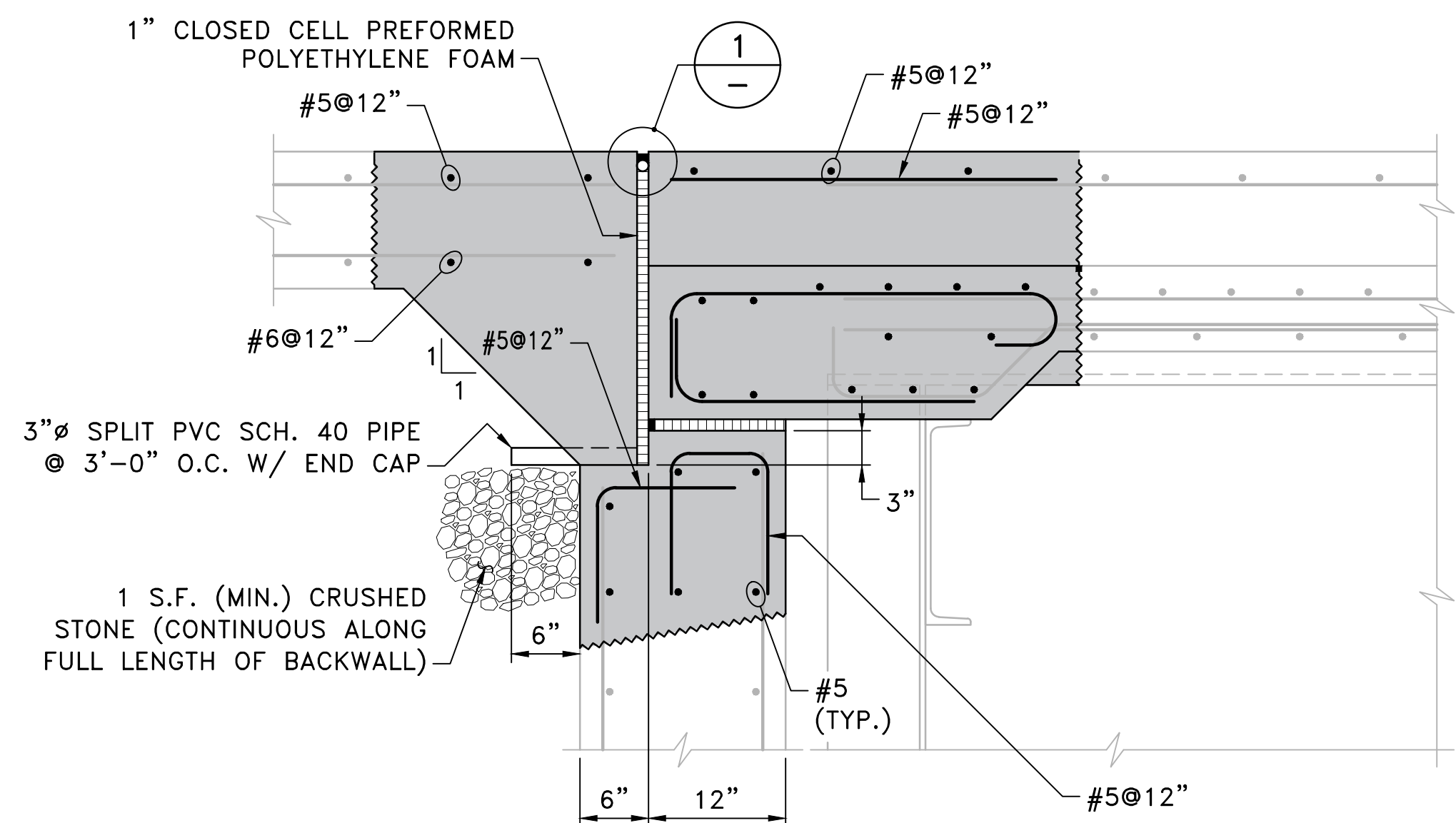
**EXISTING SAFETY WALK SECTION AT ABUTMENT**  
SCALE: 1" = 1'-0"



**PROPOSED BARRIER SECTION AT ABUTMENT**  
SCALE: 1" = 1'-0"



**DETAIL 1**  
SCALE: 3" = 1'-0"



**PROPOSED SAFETY WALK SECTION AT ABUTMENT**  
SCALE: 1" = 1'-0"



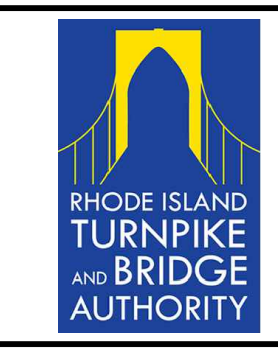
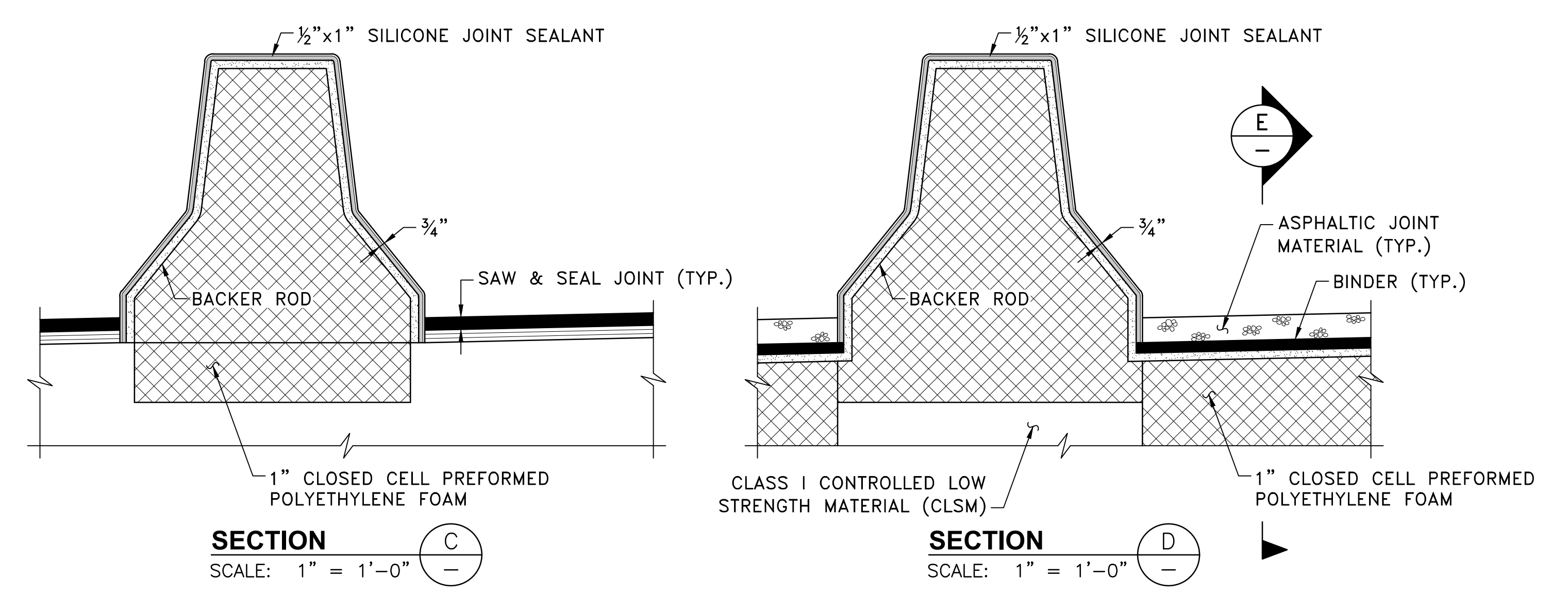
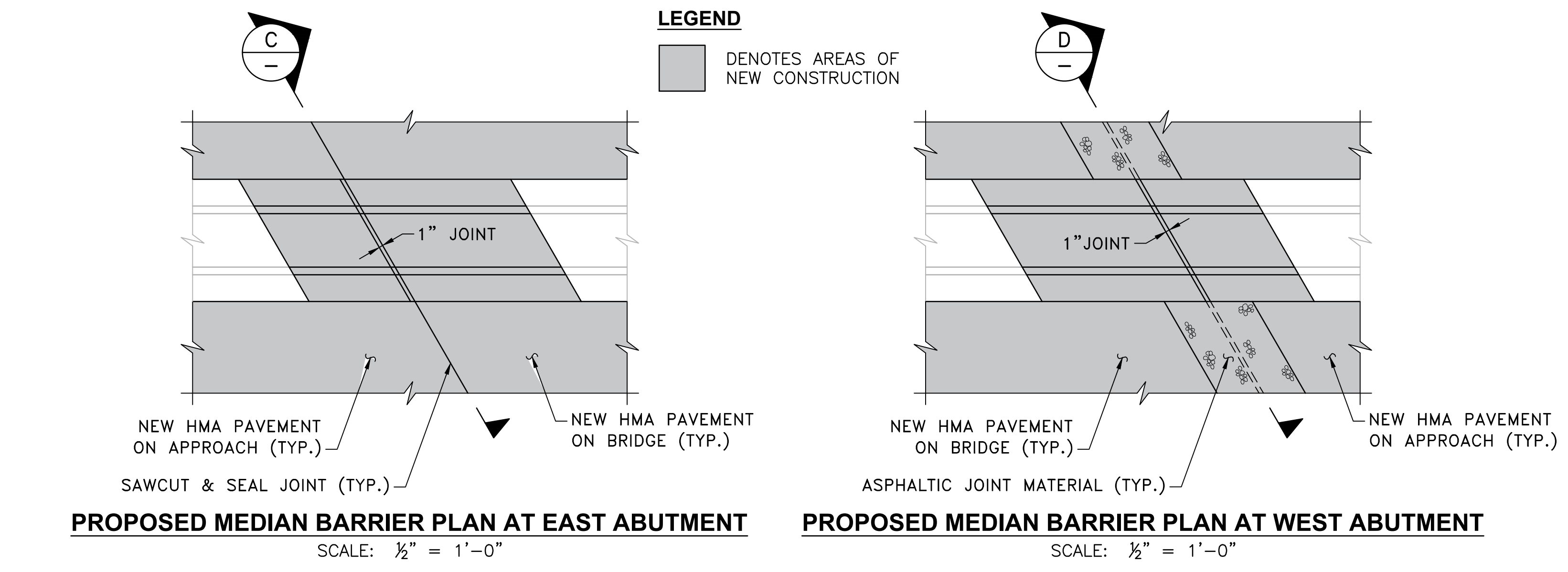
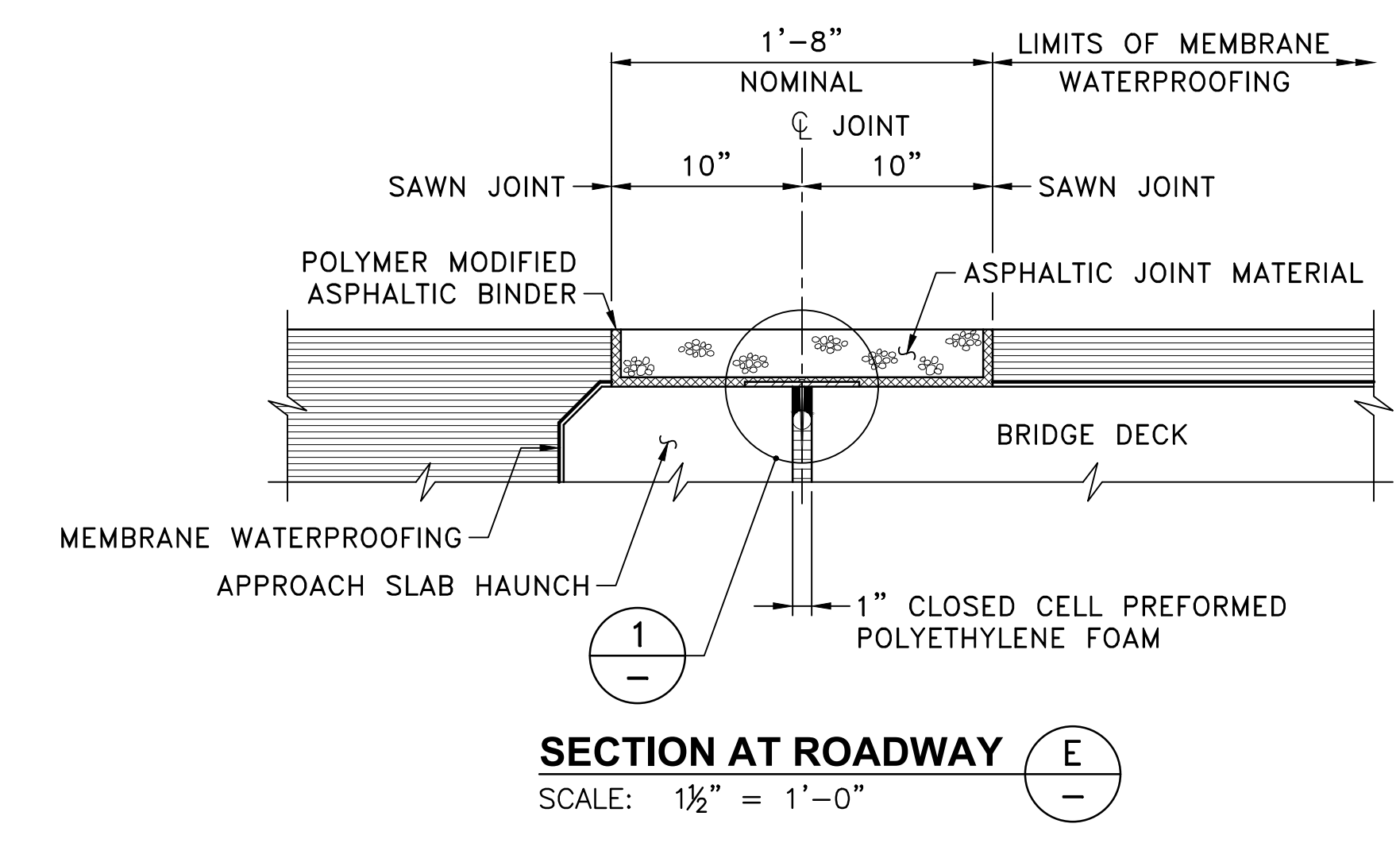
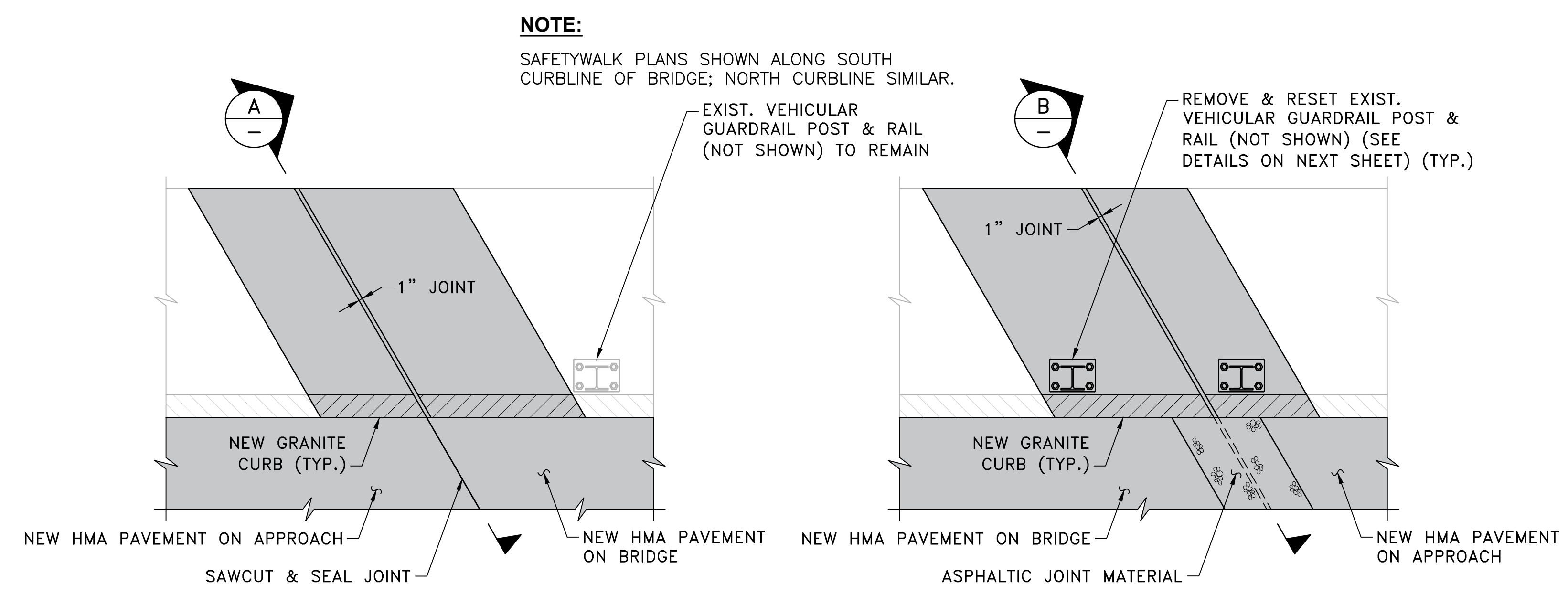
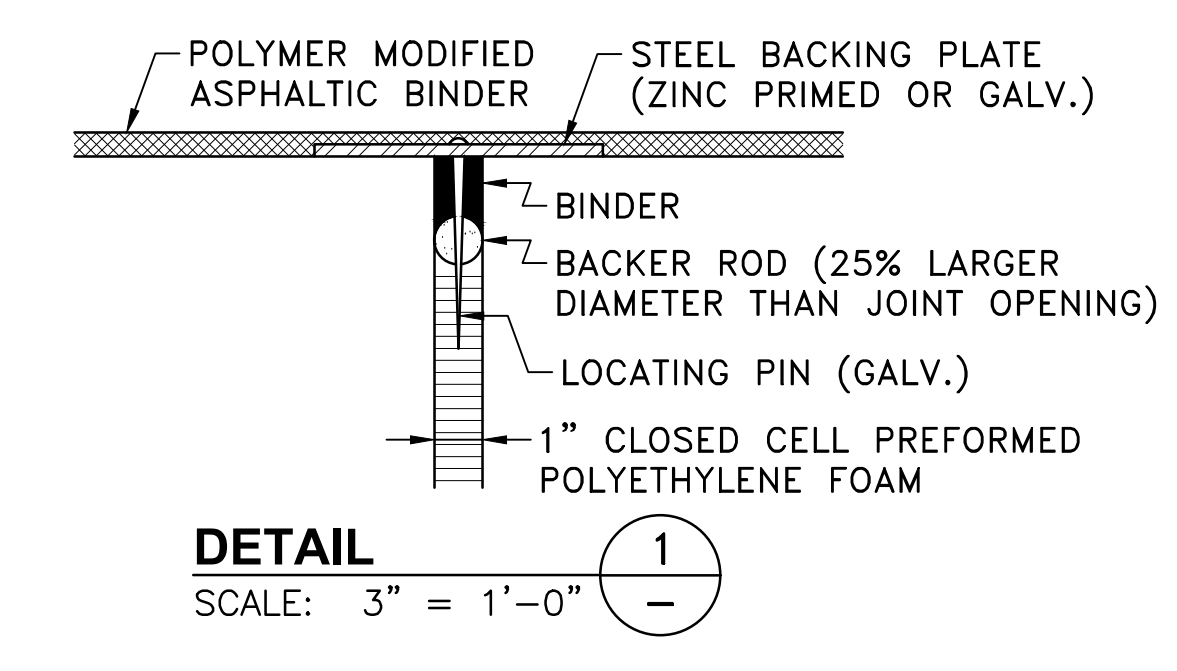
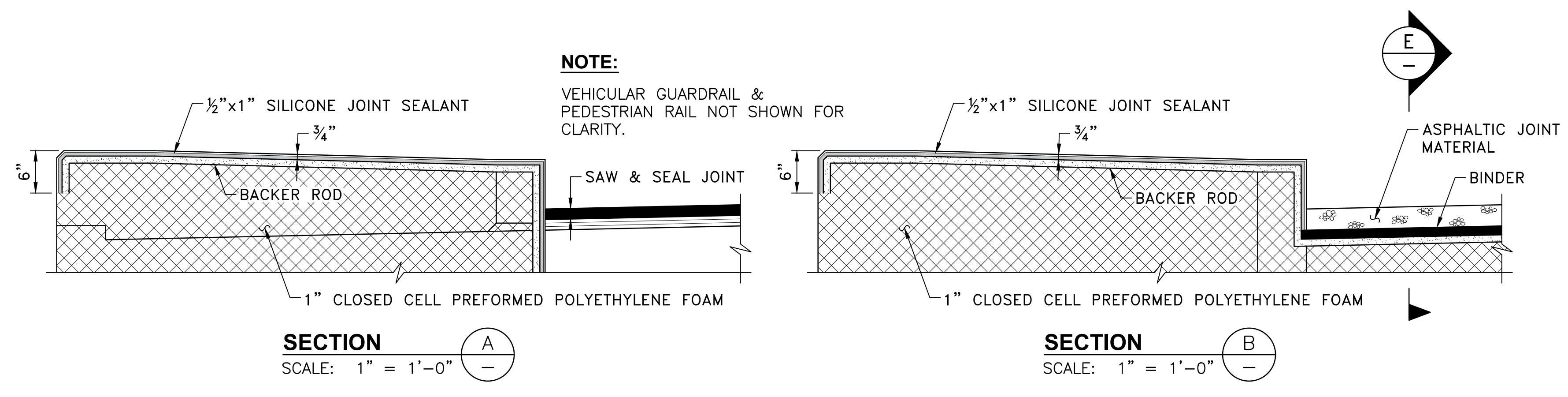
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**JOINT DETAILS - 2**



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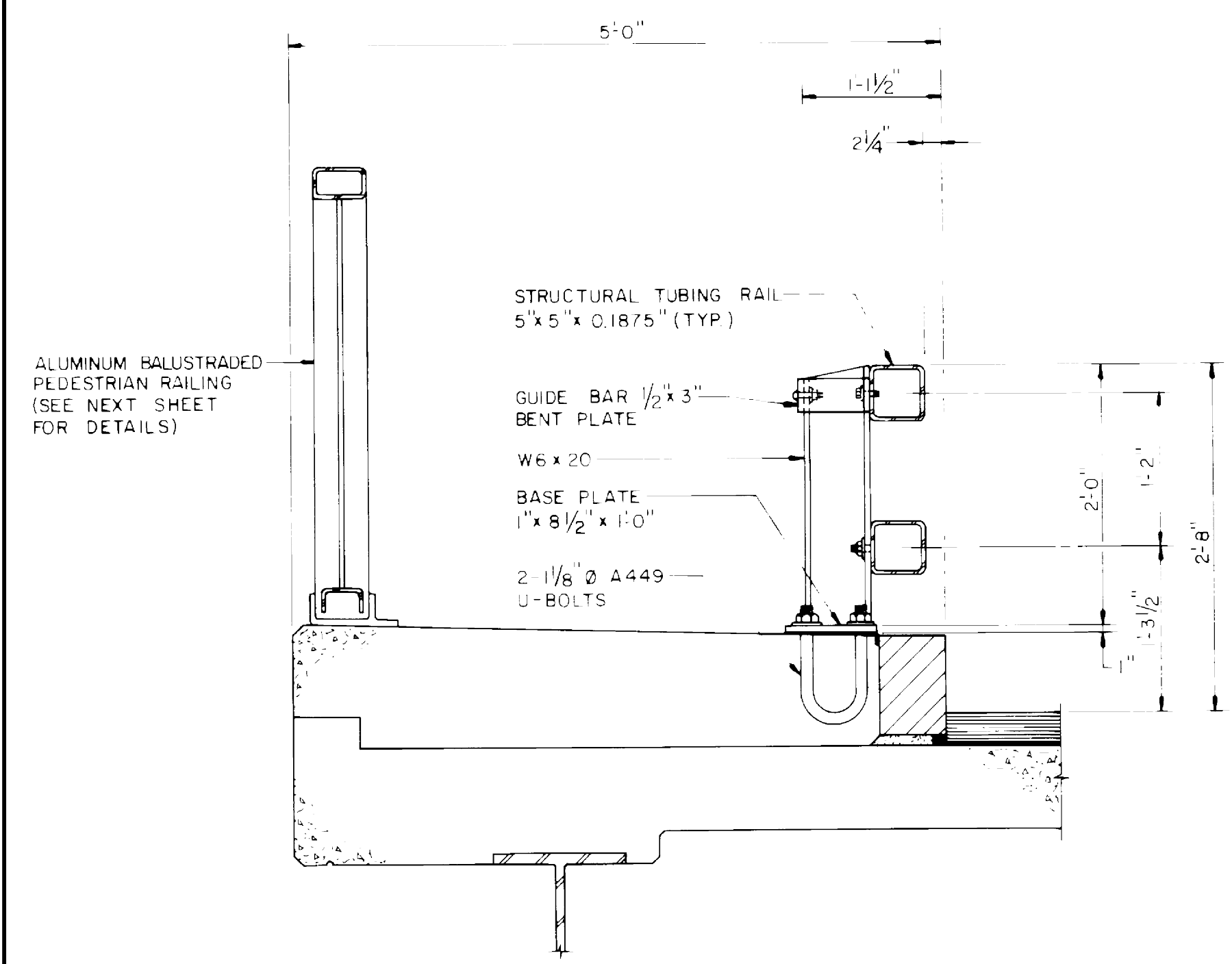
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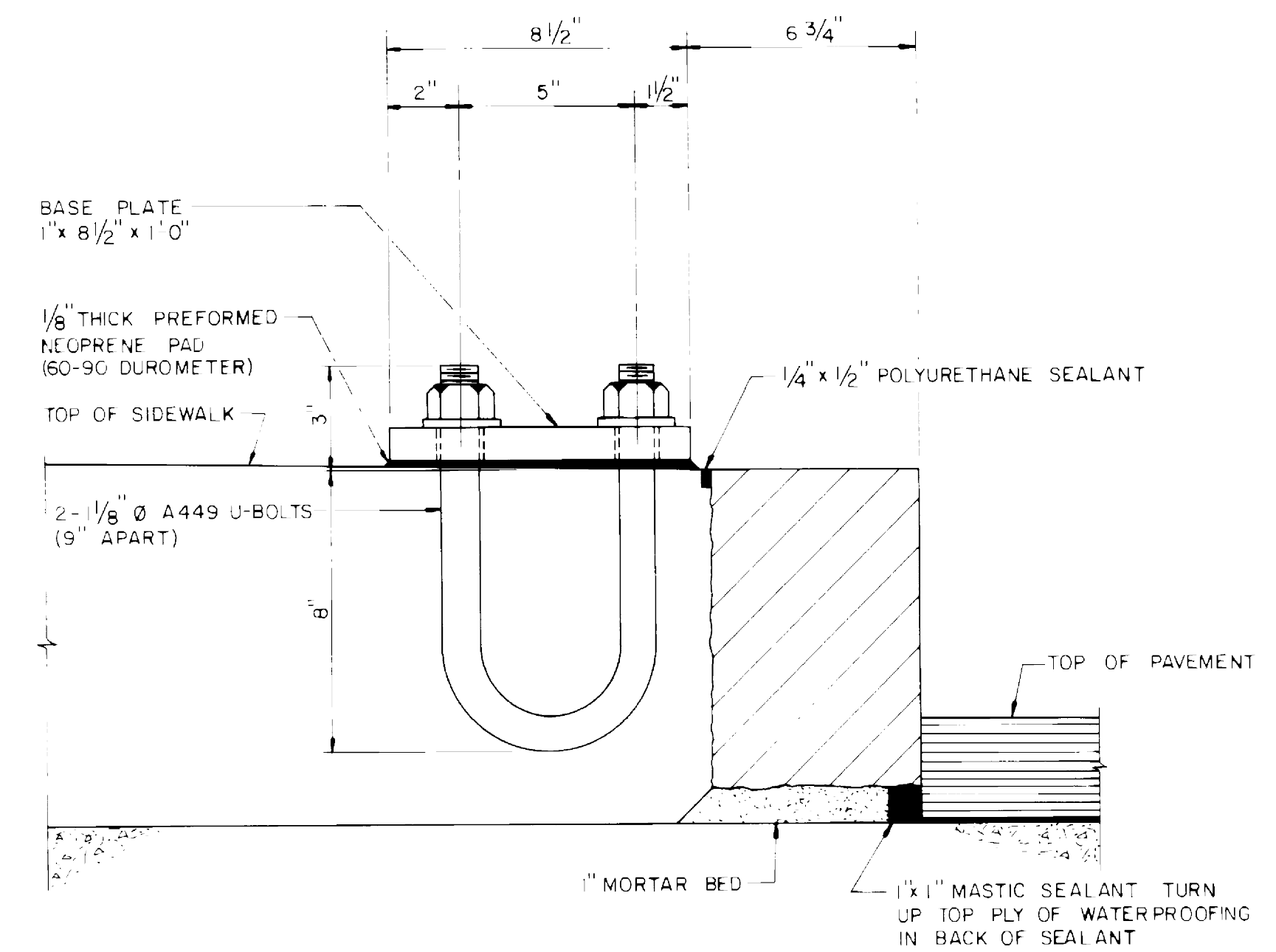
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**JOINT DETAILS - 3**

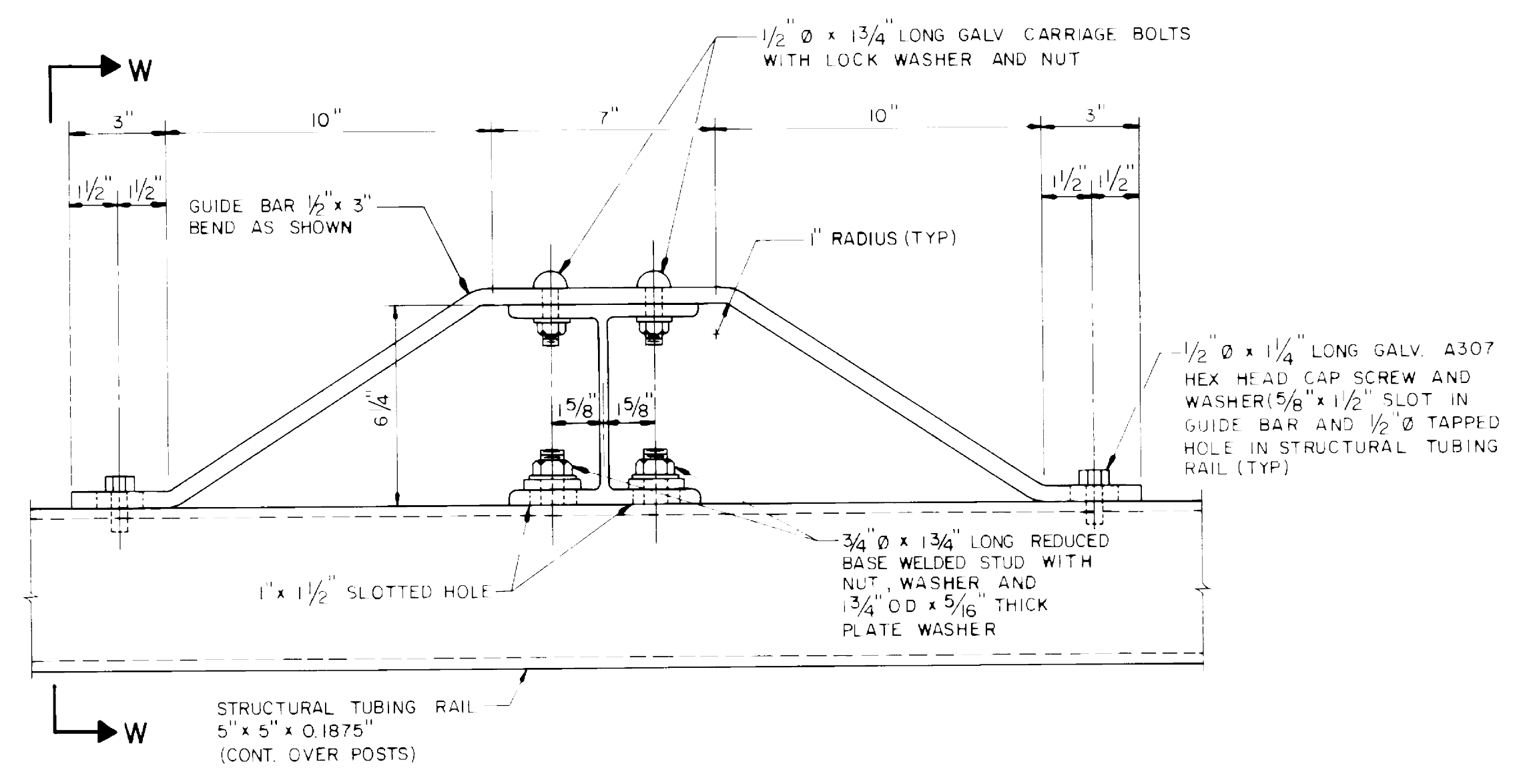




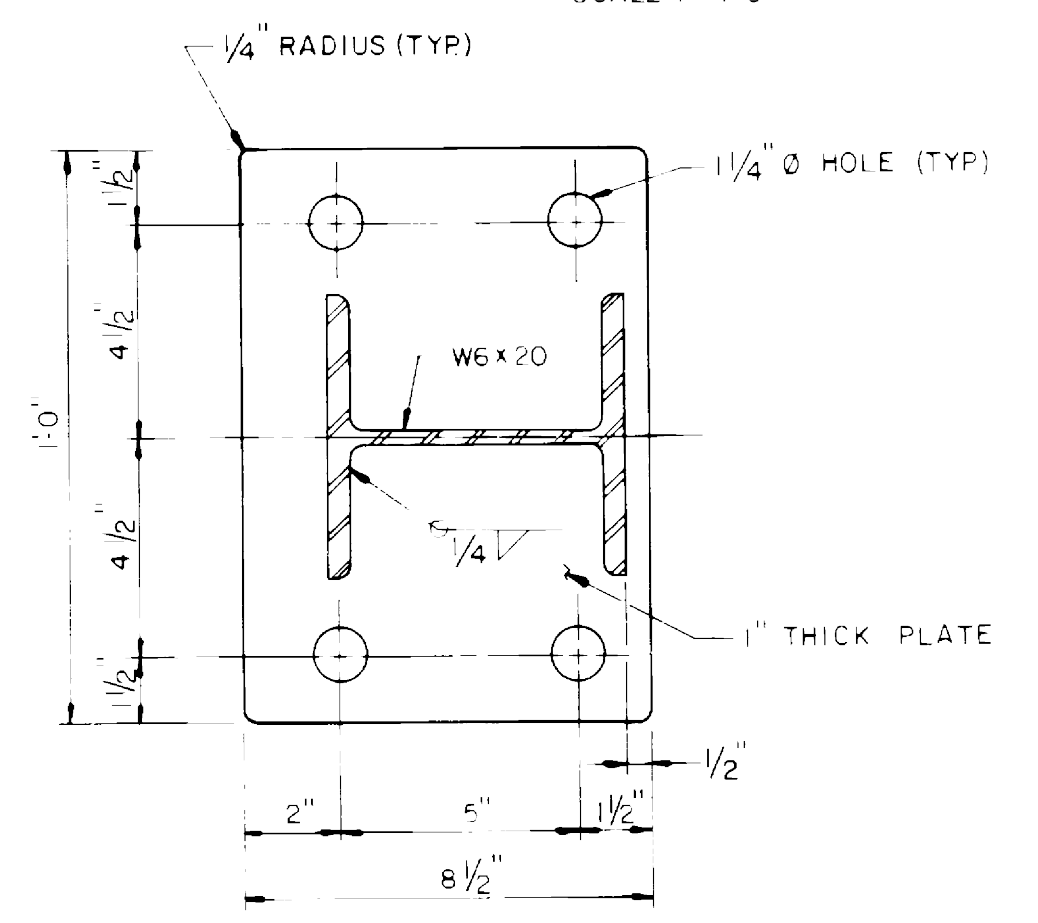
**TYPICAL SIDEWALK AND RAILING SECTION**  
SCALE 1"=1'-0"



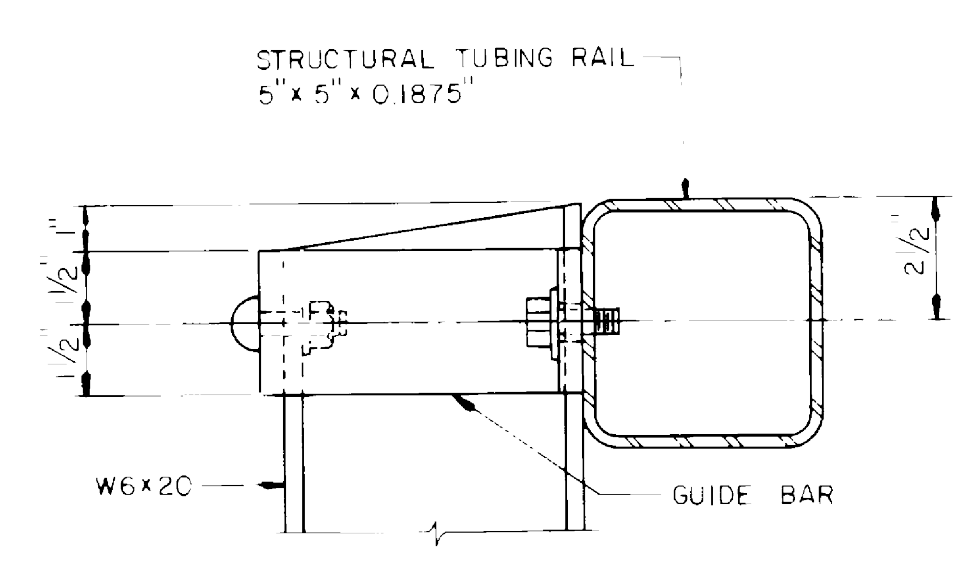
**ANCHORAGE DETAIL**  
SCALE 3"=1'-0"



**GUIDE BAR DETAIL**  
SCALE 3"=1'-0"

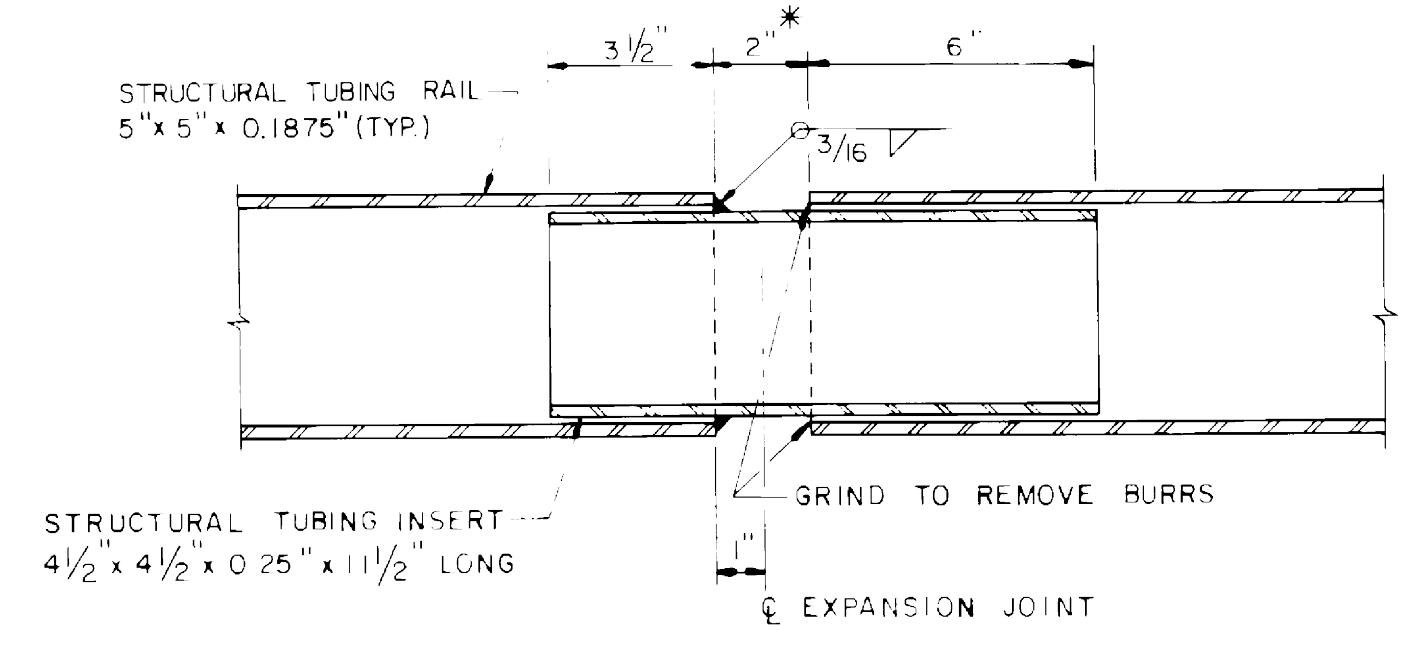


**BASE PLATE DETAIL**  
SCALE 3"=1'-0"

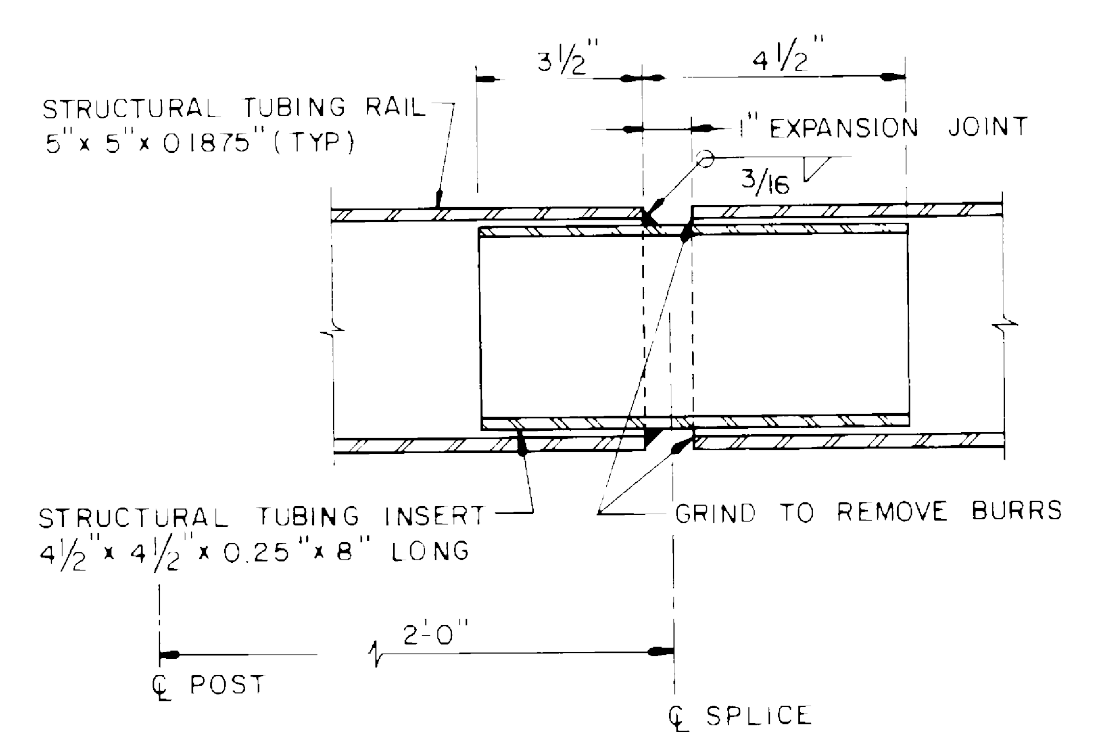


**SECTION W-W**  
SCALE 3"=1'-0"

\* ASSUMING SPLICE CONNECTION ASSEMBLED AT TEMPERATURE OF 55°F CONTRACTOR SHALL ADJUST FOR OTHER TEMPERATURES



**RAIL SPLICE AT BRIDGE EXPANSION JOINT DETAIL**  
SCALE 3"=1'-0"



**INTERMEDIATE RAIL SPLICE DETAIL**  
SCALE 3"=1'-0"

**VEHICULAR GUARDRAIL NOTES**

- EXISTING DETAILS SHOWN ON THIS SHEET ARE PROVIDED FOR REFERENCE ONLY AND ARE INTENDED TO AID THE CONTRACTOR IN THE RESETTING/RELOCATING OF RAIL POSTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTING DETAILS IN THE FIELD, INCLUDING MATERIAL PROPERTIES.
- SEE GENERAL PLAN FOR LOCATIONS.



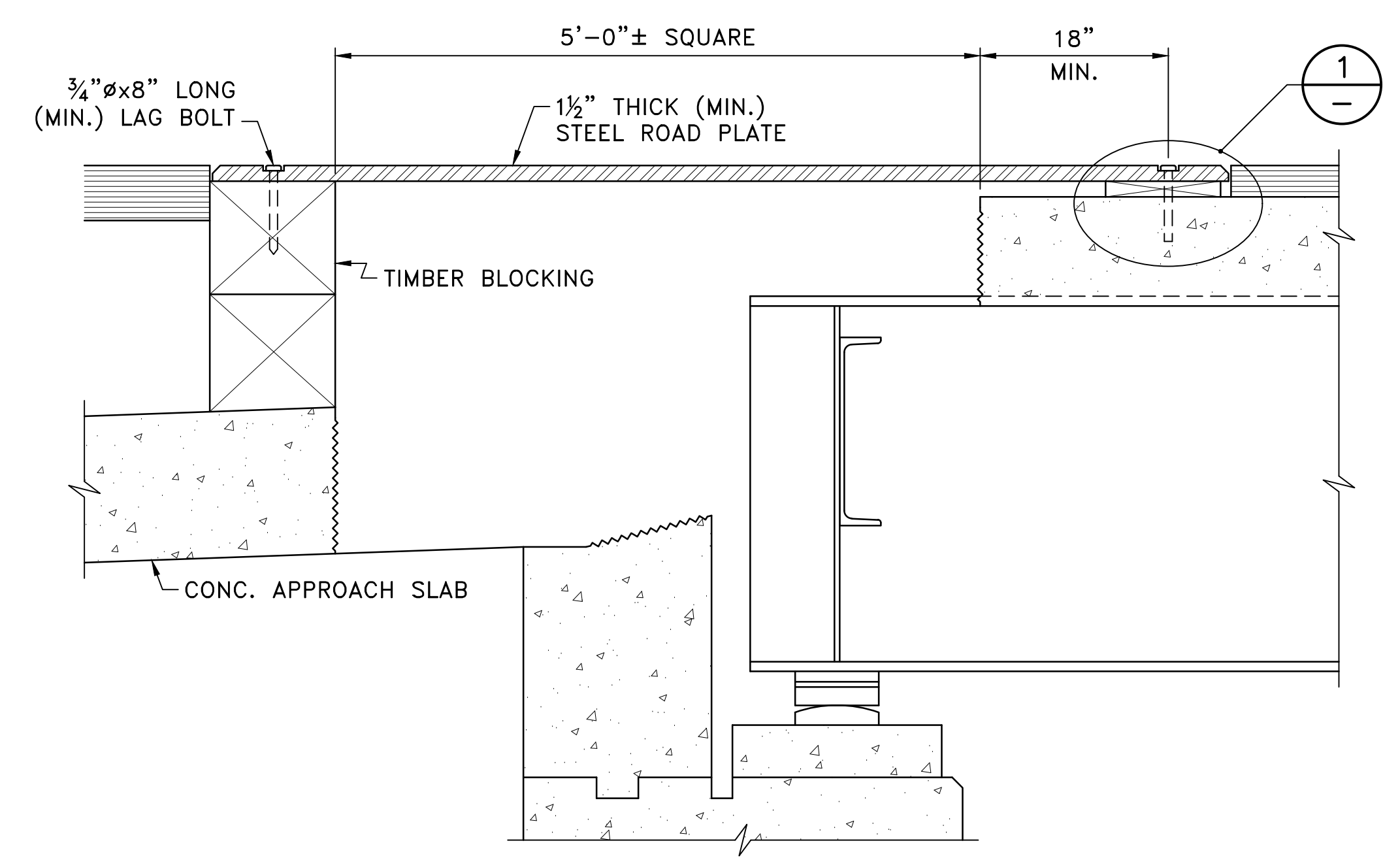
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**EXISTING VEHICULAR GUARDRAIL DETAILS**

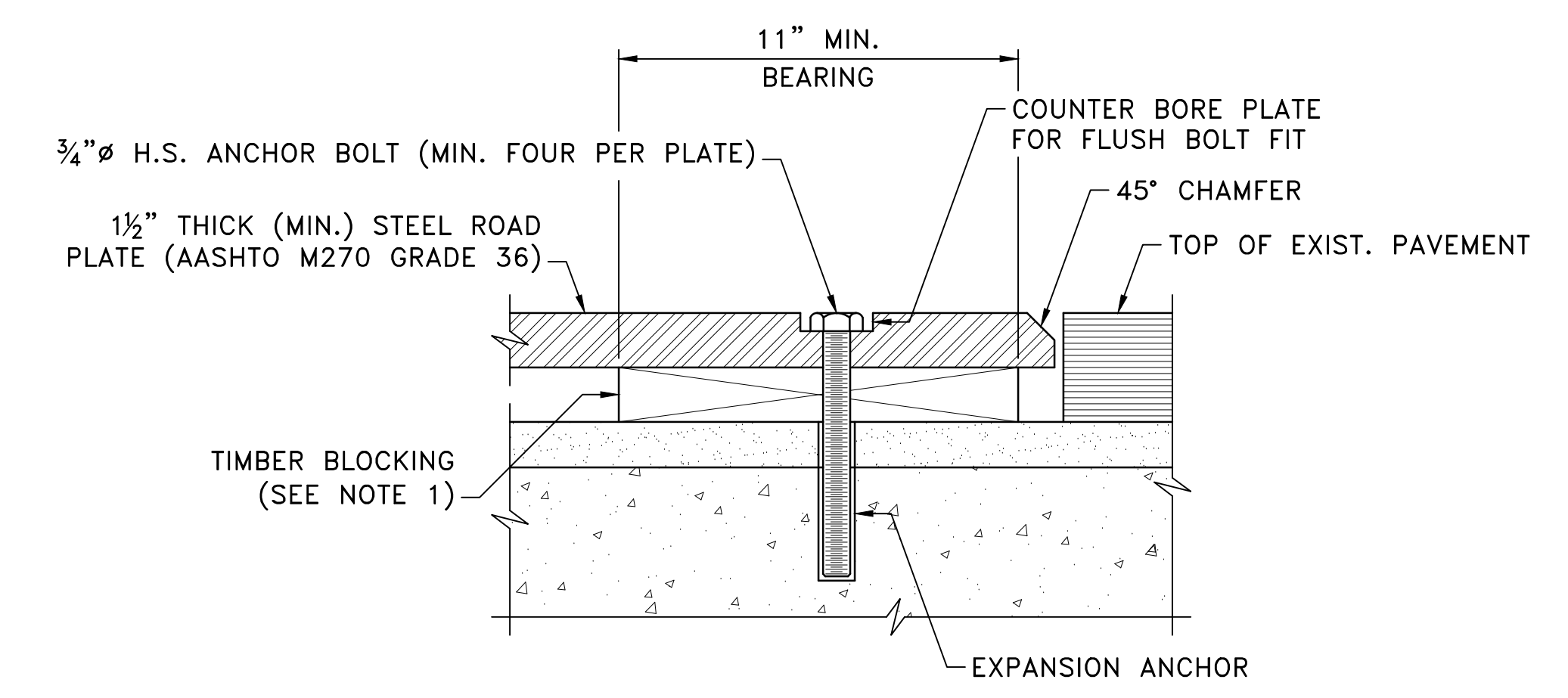


**NOTES:**

1. FINAL DESIGN OF THE STEEL ROAD PLATES SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AS APPROVED BY THE ENGINEER.
2. SECTION IS DRAWN LOOKING PARALLEL TO THE CENTERLINE OF BEARINGS. THE CONTRACTOR SHALL NOTE THAT THE CENTERLINE OF BEARINGS IS SKEWED IN RELATIONSHIP TO THE CENTERLINE OF THE ROADWAY AND SHALL TAKE THIS INTO ACCOUNT IN THE DESIGN/FABRICATION/INSTALLATION OF THE TRAFFIC PLATES. NO GAP SHALL BE ALLOWED AT THE CURBLINE.
3. STEEL ROAD PLATES SHALL FOLLOW THE CROSS SLOPE OF THE ROADWAY.
4. WIDTH OF THE PLATES SHALL BE DETERMINED BY THE CONTRACTOR BUT SHALL NOT BE LESS THAN 6 FEET.
5. TEMPORARY SHORING AND BRACING SHALL BE INSTALLED TO PREVENT UNDERMINING OF THE EXISTING APPROACH ROADWAY. THE COST OF THIS ITEM SHALL BE INCLUDED UNDER THE COST OF THE EXCAVATION.

**STEEL ROAD PLATE DETAIL**

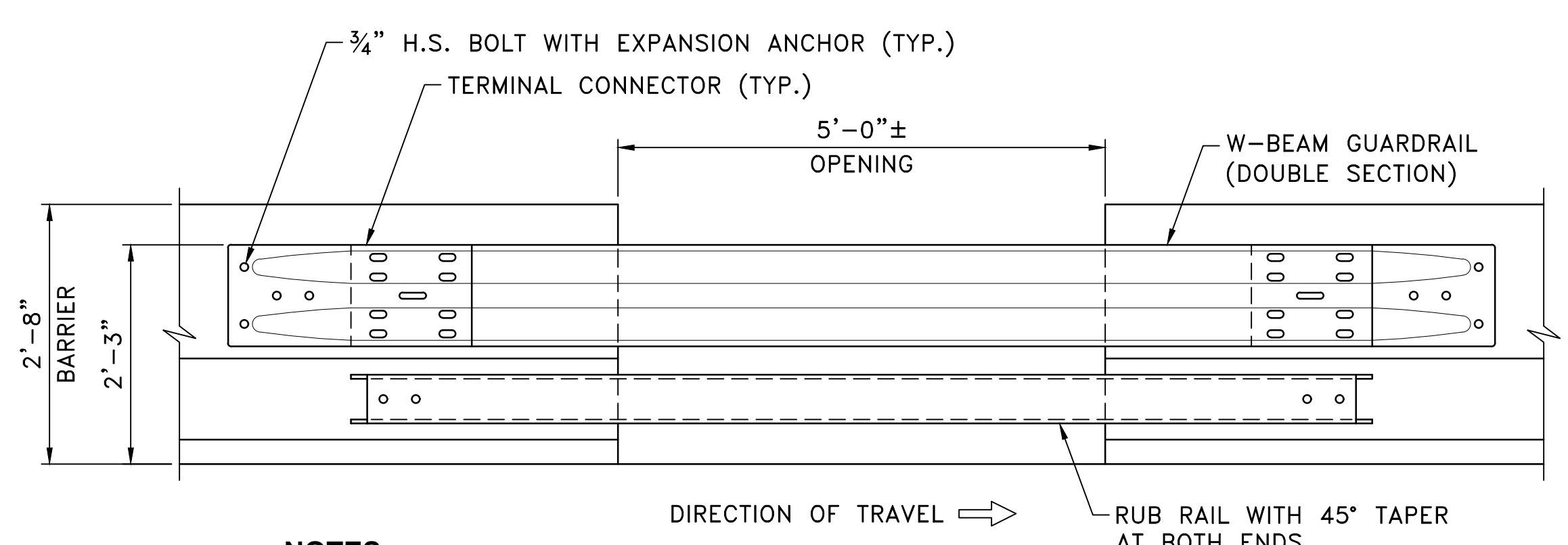
SCALE: 1" = 1'-0"



**NOTES:**

1. PROVIDE CONTINUOUS BLOCKING AS REQUIRED TO PROTECT WORK BENEATH PLATE FROM TRAFFIC DEFLECTIONS AND TO ENSURE SMOOTH TRANSITION TO ADJACENT PAVEMENT.
2. EXPANSION ANCHORS SHALL BE REPLACED AFTER REMOVING AND RE-INSTALLING ANCHOR BOLTS A MAXIMUM OF SEVEN (7) TIMES.
3. UPON COMPLETION OF THE WORK, THE EXPANSION ANCHORS SHALL BE REMOVED AND THE HOLES FILLED WITH NON-SHRINK GROUT TO THE SATISFACTION OF THE ENGINEER.

**DETAIL** 1  
SCALE: 3" = 1'-0"

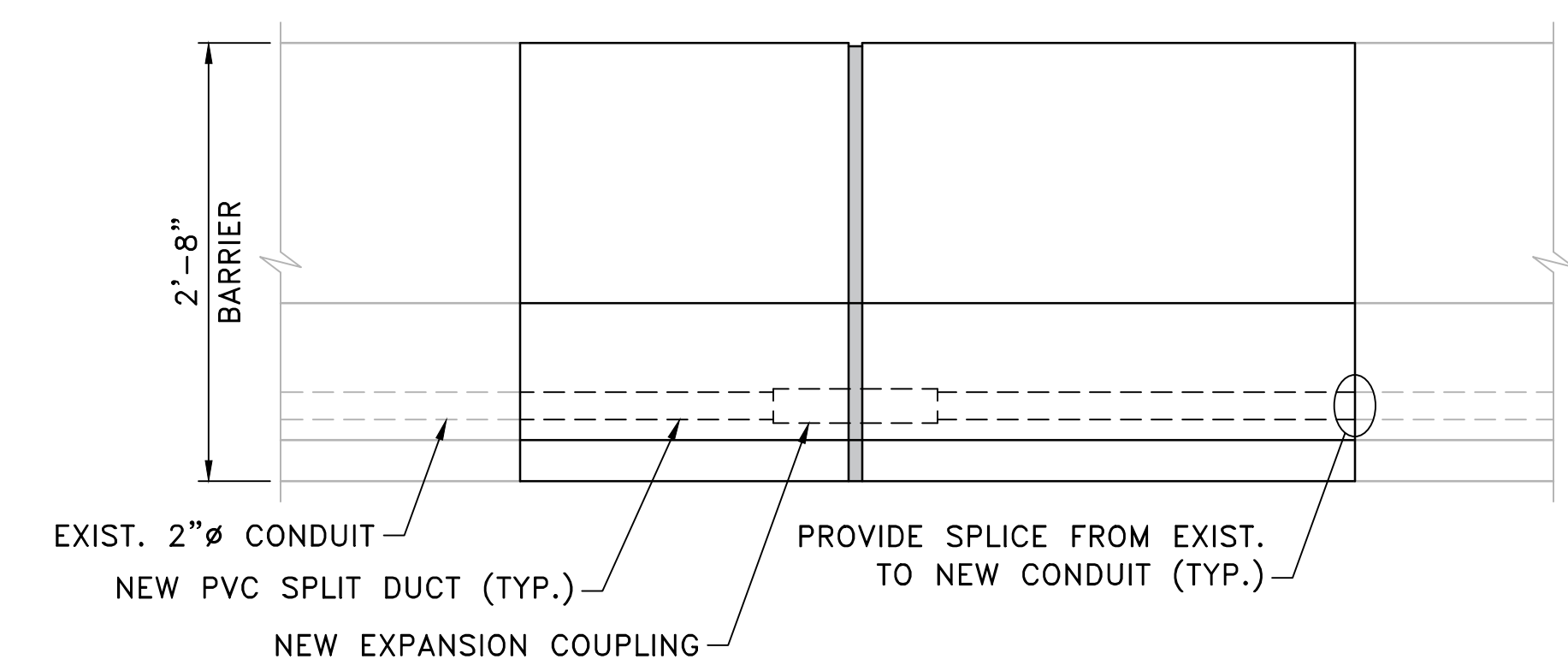


**NOTES:**

1. GUARDRAIL PROTECTION SHALL BE IN PLACE WHEN TRAFFIC IS ALLOWED ADJACENT TO THE BARRIER.
2. EXPANSION ANCHORS SHALL BE REPLACED AFTER REMOVING AND RE-INSTALLING ANCHOR BOLTS A MAXIMUM OF SEVEN (7) TIMES.
3. UPON COMPLETION OF THE WORK, THE EXPANSION ANCHORS SHALL BE REMOVED AND THE HOLES FILLED WITH NON-SHRINK GROUT TO THE SATISFACTION OF THE ENGINEER.

**MEDIAN BARRIER PROTECTION DURING CONSTRUCTION**

SCALE: 3/4" = 1'-0"



**NOTE:**

ALL COSTS ASSOCIATED WITH THE NEW CONDUIT/DUCT WORK INCLUDING, BUT NOT LIMITED TO, SPLICES, SPLIT DUCT & EXPANSION COUPLINGS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE BARRIER CONCRETE; NO ADDITIONAL PAYMENT WILL BE MADE.

**MEDIAN BARRIER ELEVATION AT JOINT**

SCALE: 1" = 1'-0"

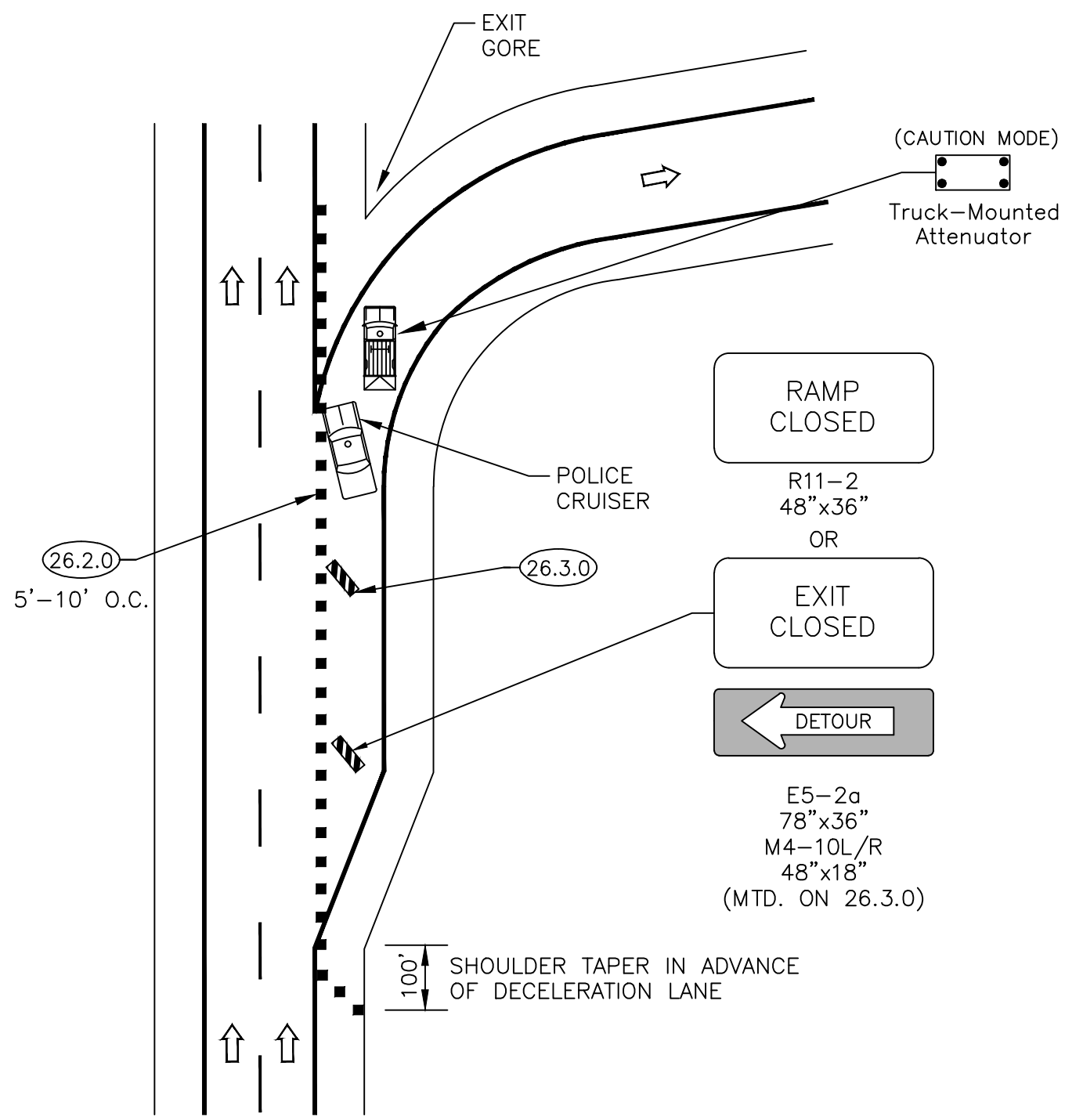


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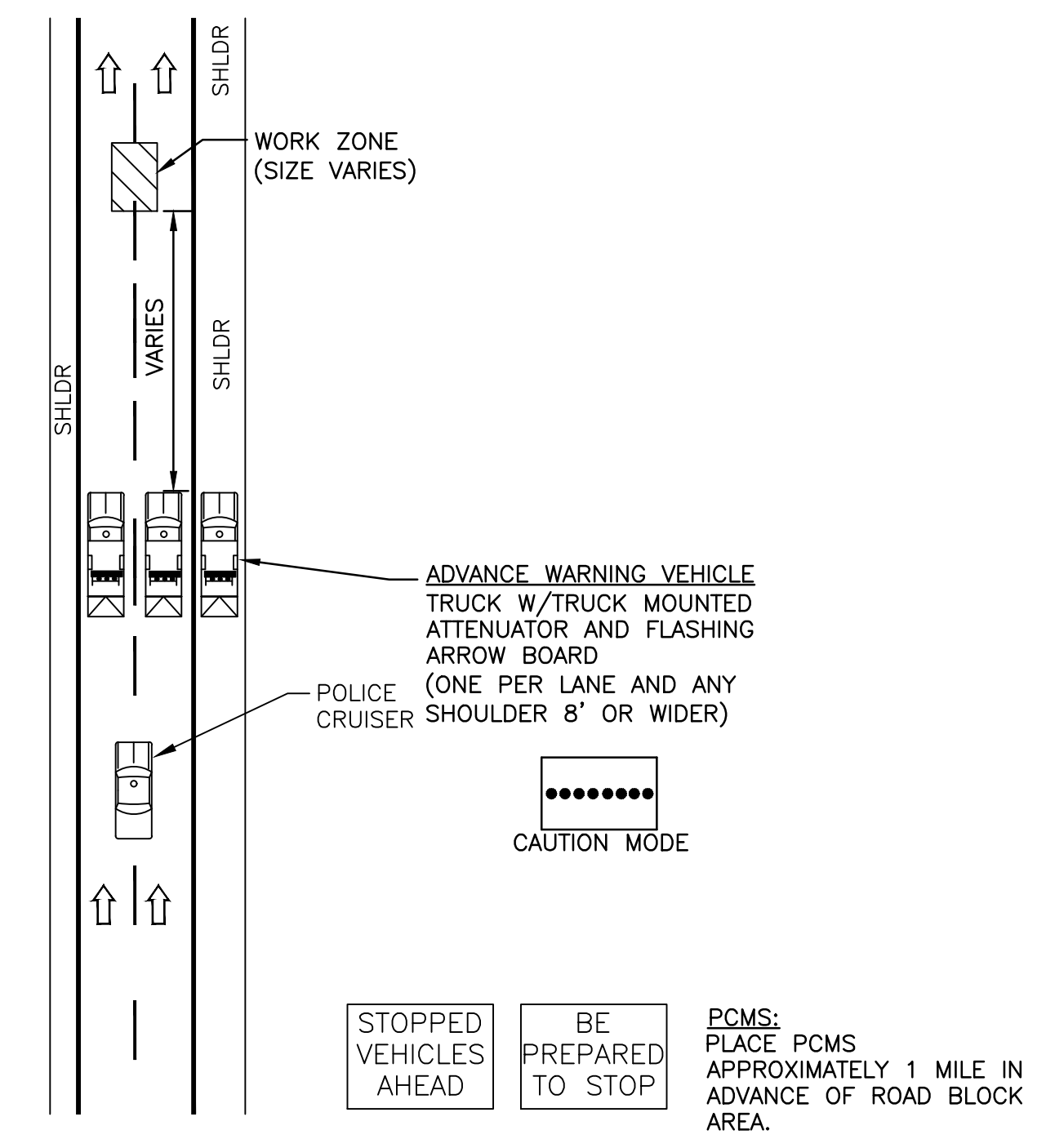
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**MISCELLANEOUS DETAILS**



- NOTES**
- RAMPS SHALL BE CLOSED FROM THE BEGINNING OF THE DECELERATION LANE TO A POINT BEYOND THE EXIT GORE.
  - A TRAFFIC PERSON WITH A POLICE VEHICLE SHALL BE USED AT EACH RAMP CLOSURE.
  - ADDITIONAL R.I. STD. 26.3.0 MAY BE USED.

**TYPICAL OFF RAMP CLOSURE**

- NOTES**
- ANY ON-RAMP(S) WITHIN THE ROAD BLOCK ZONE SHALL ALSO HAVE A TRUCK W/TRUCK MOUNTED ATTENUATOR AND FLASHING ARROW BOARD TO HOLD TRAFFIC. ADDITIONAL TTC DEVICES SHALL BE PLACED AS NEEDED.
  - ALL VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
  - POLICE VEHICLES SHALL DISPLAY FULL EMERGENCY LIGHTING AND REQUIRE A LIGHT BEAR ON THE ROOF OF THE VEHICLE.
  - ROLLING ROADBLOCKS SHALL BE APPROVED BY RIDOT PRIOR TO USE AND SUBJECT TO TIME LIMITATIONS AS SET FORTH BY RIDOT INCLUDING FREQUENCY, DURATION, AND TIME PERIOD(S).

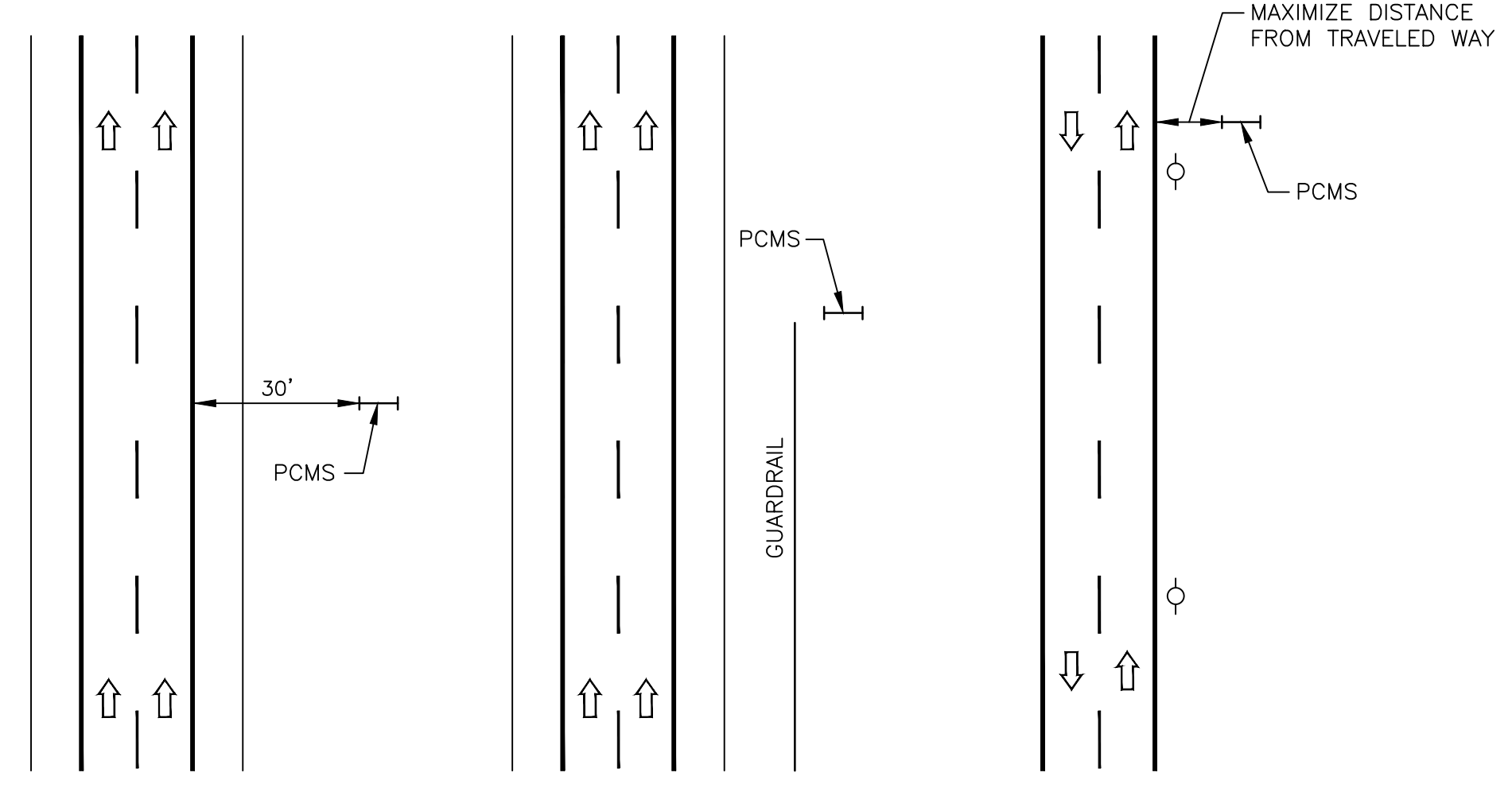


**ROLLING ROADBLOCK CLOSURE ON MULTI-LANE HIGHWAY**

**MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:**

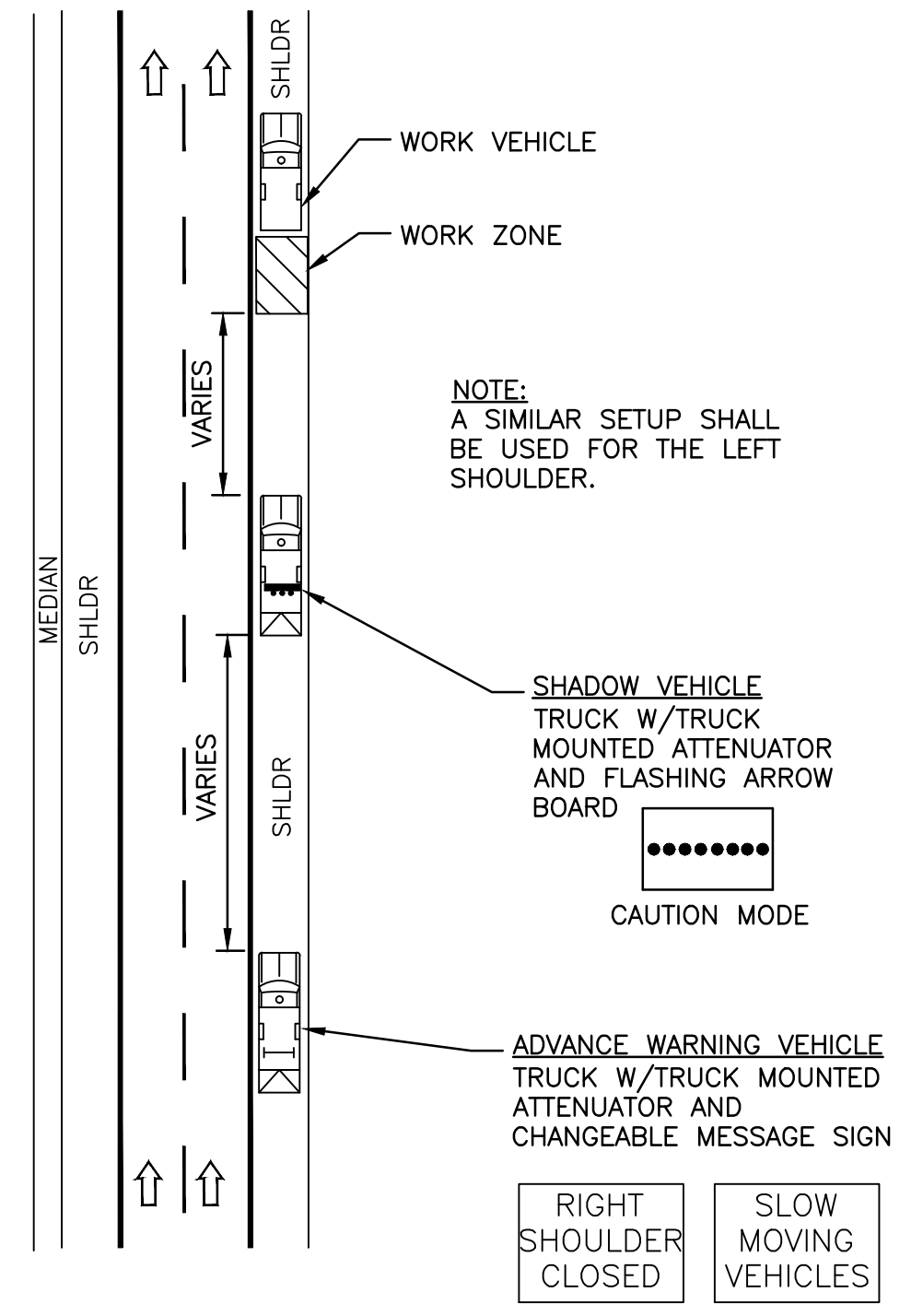
**THE FOLLOWING NOTES ARE IN ADDITION TO THOSE LISTED ON GENERAL NOTES - 2 OF THE PLAN SET.**

- REPLACE STANDARD NOTES-2 NOTE 1 WITH THE FOLLOWING: ALL TEMPORARY TRAFFIC CONTROL SET-UPS AND DEVICES AND THEIR INSTALLATION, MAINTENANCE, AND REMOVAL SHALL CONFORM TO THE LATEST EDITION OF "THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) WITH ALL REVISIONS, AND THE LATEST EDITION OF THE "RIDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" WITH ALL REVISIONS.
- DISTANCES ARE A GUIDE AND MAY BE ADJUSTED IN THE FIELD BY THE ENGINEER.
- MAXIMUM SPACING OF CHANNELIZATION DEVICES IN A TAPER IS EQUAL IN FEET TO THE SPEED LIMIT IN MPH. MAXIMUM SPACING OF CHANNELIZATION DEVICES IN A TANGENT SECTION IS EQUAL IN FEET TO TWO TIMES THE SPEED LIMIT IN MPH.
- MINIMUM LANE WIDTH IS TO BE 11 FEET UNLESS OTHERWISE SHOWN OR CALLED FOR IN THE GENERAL RESTRICTION CHART(S) OF THE TMP. MINIMUM LANE WIDTH TO BE MEASURED FROM THE EDGE OF CHANNELIZATION DEVICES OR TEMPORARY BARRIER.
- THE DISTANCE BETWEEN THE SHADOW VEHICLE AND THE WORK SPACE SHOULD BE SELECTED BASED ON TRAFFIC AND SITE CONDITIONS AS WELL AS THE CHARACTERISTICS OF THE SHADOW VEHICLE/ATTENUATOR AND ITS MANUFACTURER'S RECOMMENDATIONS, BUT SHOULD BE NO GREATER THAN THE MINIMUM DISTANCE SUFFICIENT TO ENSURE THAT THE SHADOW VEHICLE WILL NOT ROLL INTO THE WORK SPACE WHEN HIT BY AN ERRANT VEHICLE.
- WHERE A SIDE STREET OR RAMP INTERSECTS THE WORK ZONE, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE APPLICABLE SECTION(S) OF THE MUTCD.
- THE CONTRACTOR SHALL INSTALL AND MAINTAIN A RHODE ISLAND STANDARD 26.2.0 BARRICADE WITH APPROPRIATE MARKINGS AT EACH LOCATION WHERE ADJUSTMENT TO UTILITY STRUCTURES HAVE BEEN MADE UNTIL RESURFACING WORK HAS BEEN PERFORMED. OTHER TYPES OF PROTECTIVE DEVICES MAY BE USED IF APPROVED BY THE ENGINEER.
- WHENEVER MICROMILLING LEAVES AN UNEVEN SURFACE, A BUMP SIGN, W8-1, SHALL BE PROVIDED.
- SEE TMP PLAN IN THE CONTRACT BOOK FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS AND RESTRICTIONS.
- A MINIMUM SIGHT DISTANCE OF 425 FEET SHALL BE PROVIDED IN ADVANCE OF ALL LANE CLOSURES. THE BUFFER ZONE MAY BE EXTENDED AS NECESSARY TO PROVIDE ADEQUATE SIGHT DISTANCE. NOTE VERTICAL AND HORIZONTAL CURVES WITHIN WORK ZONE AREAS.

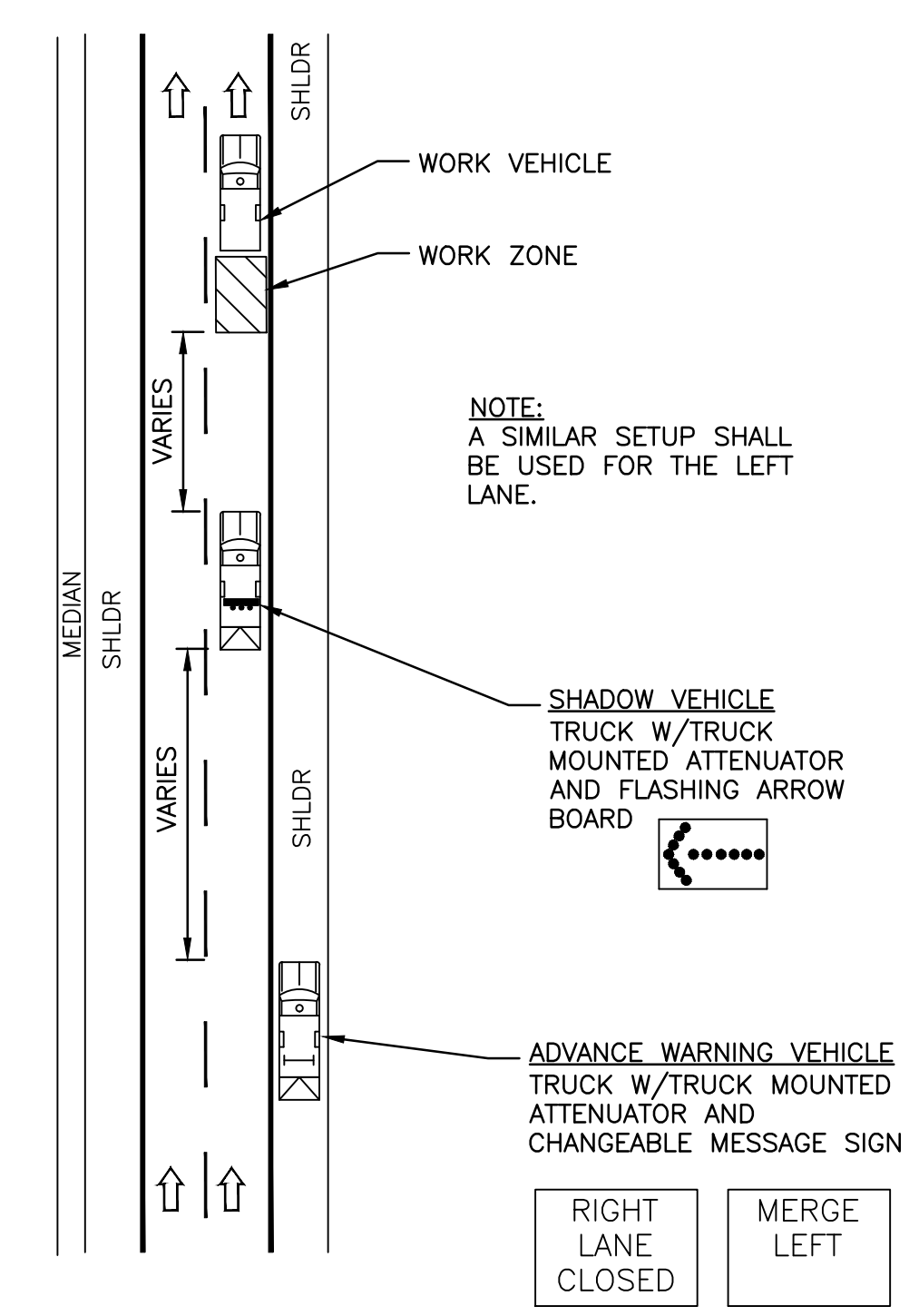


**PCMS LOCATION DETAILS**

- NOTES**
- PCMS SHALL BE USED ON MAJOR ROADS IN EACH DIRECTIONS OF TRAFFIC.
  - PCMS SHALL BE IMPLEMENTED 2 WEEKS PRIOR START OF CONSTRUCTION AND SHALL REMAIN IN PLACE UNTIL 2 WEEKS AFTER CONSTRUCTION IS COMPLETE.
  - PCMS MESSAGE WILL VARY AT THE DIRECTION OF THE ENGINEER.



**TYPICAL MOBILE OPERATION SHOULDER CLOSURE ON MULTI-LANE HIGHWAY**



**TYPICAL MOBILE OPERATION LANE CLOSURE ON MULTI-LANE HIGHWAY**

**LEGEND**

- CHANNELIZING DEVICE
- TRAFFIC CONE (R.I. STD. 26.1.0)
- DRUM BARRICADE (R.I. STD. 26.2.0)
- SIGN ON PORTABLE SIGN SUPPORT
- TYPE III BARRICADE
- FLASHING ARROW BOARD
- TRAFFIC PERSON
- WORK SPACE
- DIRECTION OF TRAVEL
- WORK VEHICLE
- TRUCK-MOUNTED ATTENUATOR
- ARROW PANEL



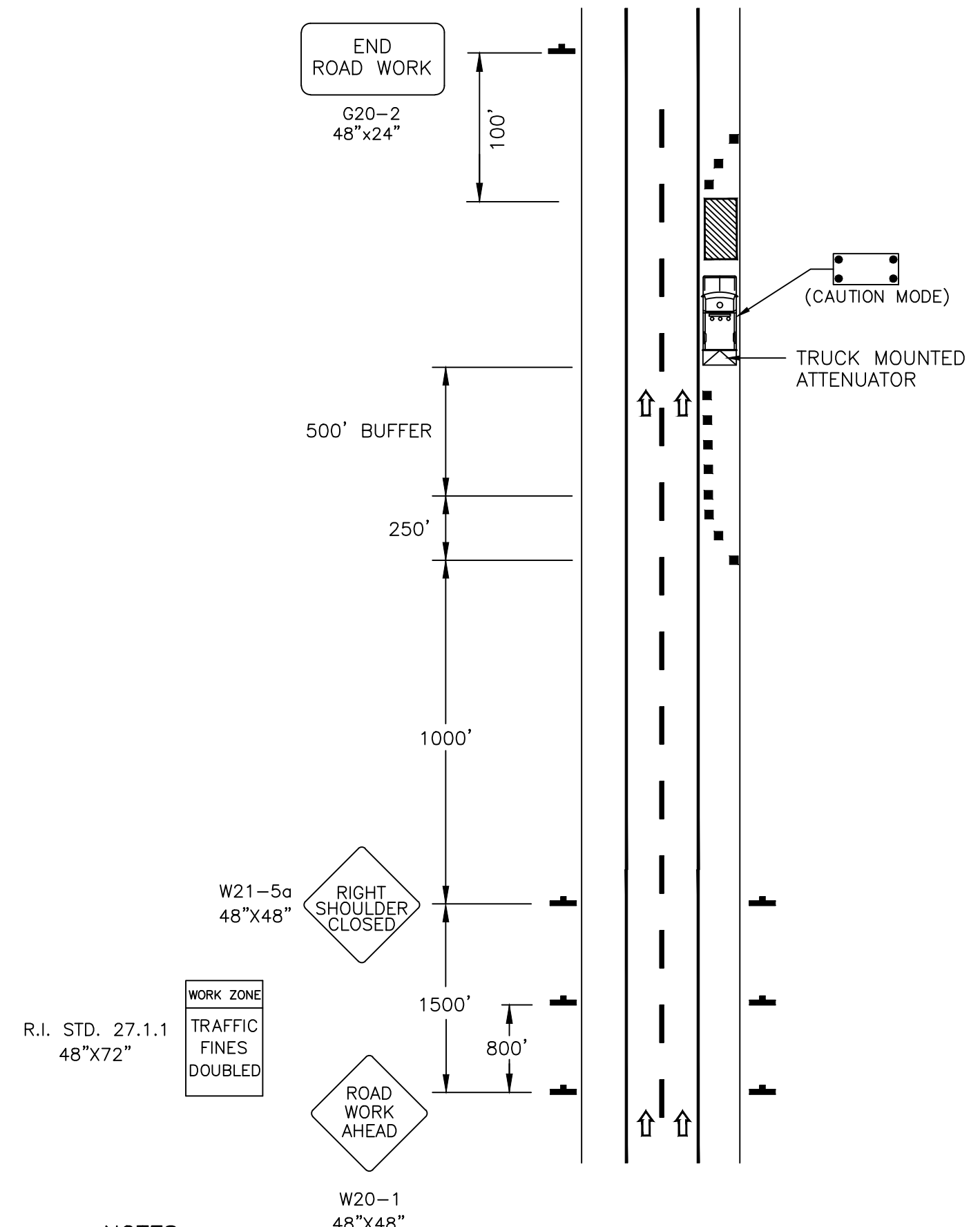
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REVISIONS			REVISIONS		
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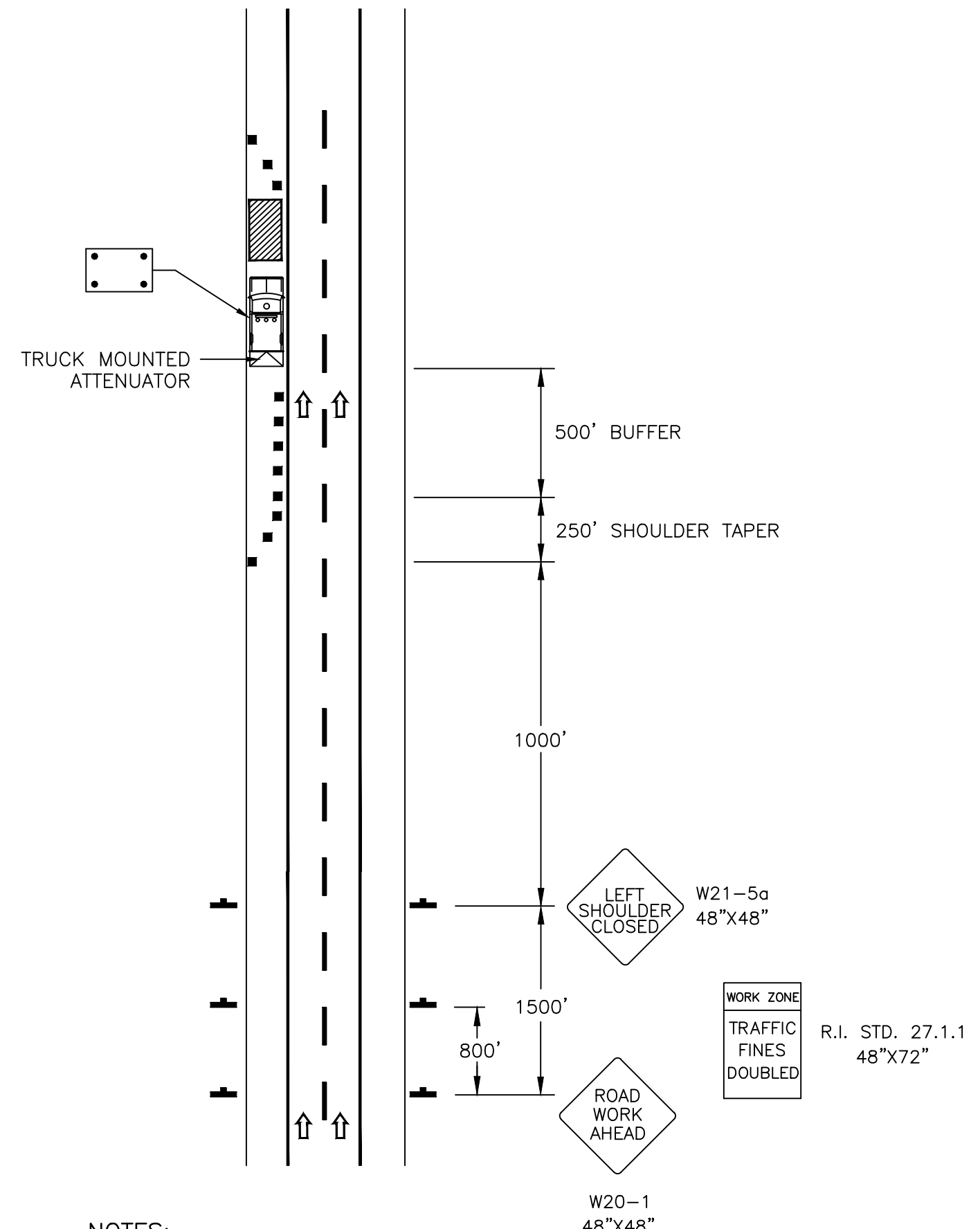
**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
**MAINTENANCE AND PROTECTION OF TRAFFIC DETAILS - 1**



**NOTES:**

1. SHOULDER CLOSED SIGNS SHOULD BE USED ON LIMITED-ACCESS HIGHWAYS WHERE THERE IS NO OPPORTUNITY FOR DISABLED VEHICLES TO PULL OFF THE ROADWAY.
2. IF DRIVER CANNOT SEE A PULL-OFF AREA BEYOND THE CLOSED SHOULDER, INFORMATION REGARDING THE LENGTH OF THE SHOULDER CLOSURE SHOULD BE PROVIDED IN FEET OR MILES AS APPROPRIATE.

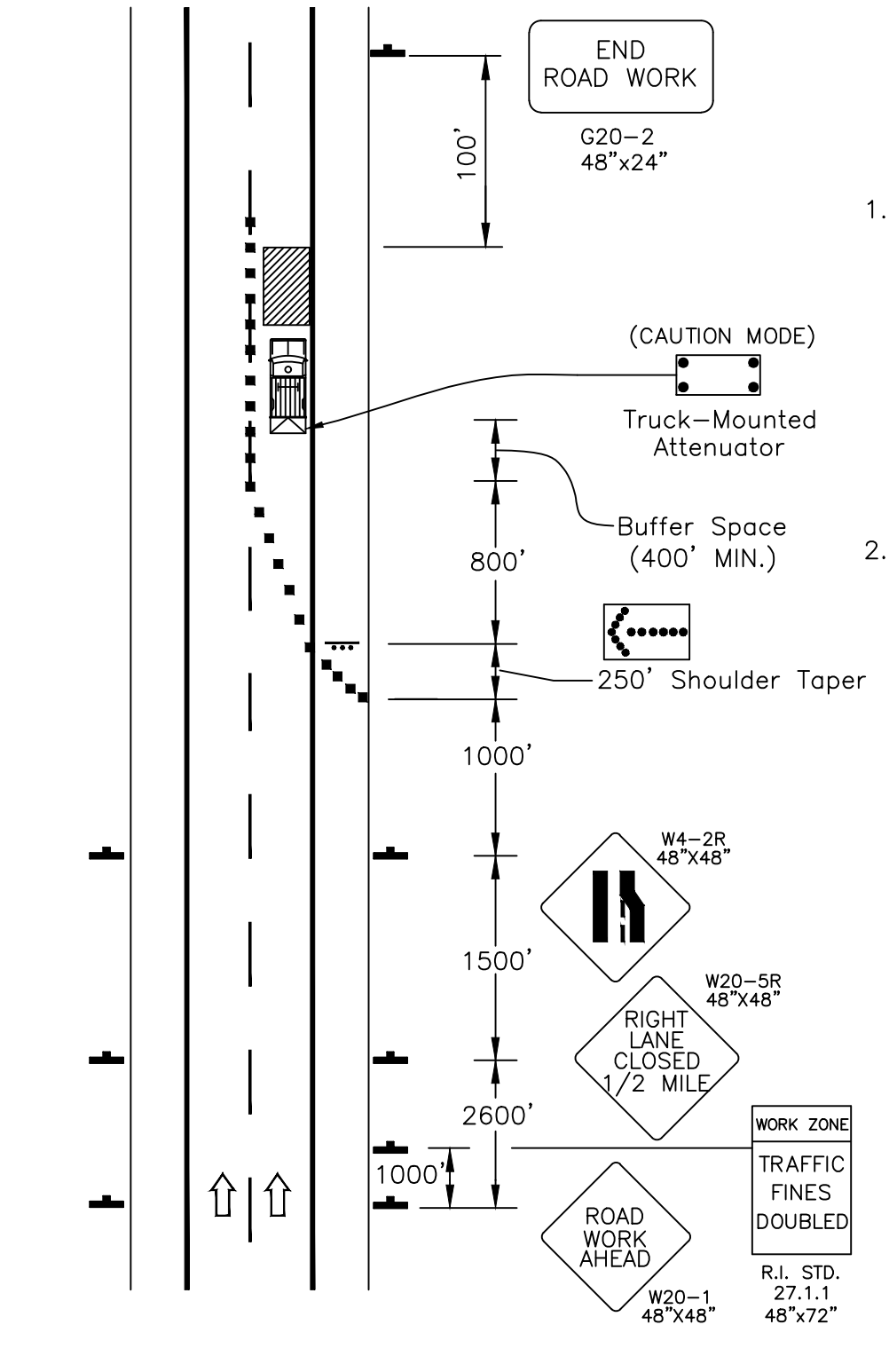
**TYPICAL RIGHT SHOULDER CLOSURE ON A FREEWAY**



**NOTES:**

1. SHOULDER CLOSED SIGNS SHOULD BE USED ON LIMITED-ACCESS HIGHWAYS WHERE THERE IS NO OPPORTUNITY FOR DISABLED VEHICLES TO PULL OFF THE ROADWAY.
2. IF DRIVER CANNOT SEE A PULL-OFF AREA BEYOND THE CLOSED SHOULDER, INFORMATION REGARDING THE LENGTH OF THE SHOULDER CLOSURE SHOULD BE PROVIDED IN FEET OR MILES AS APPROPRIATE.

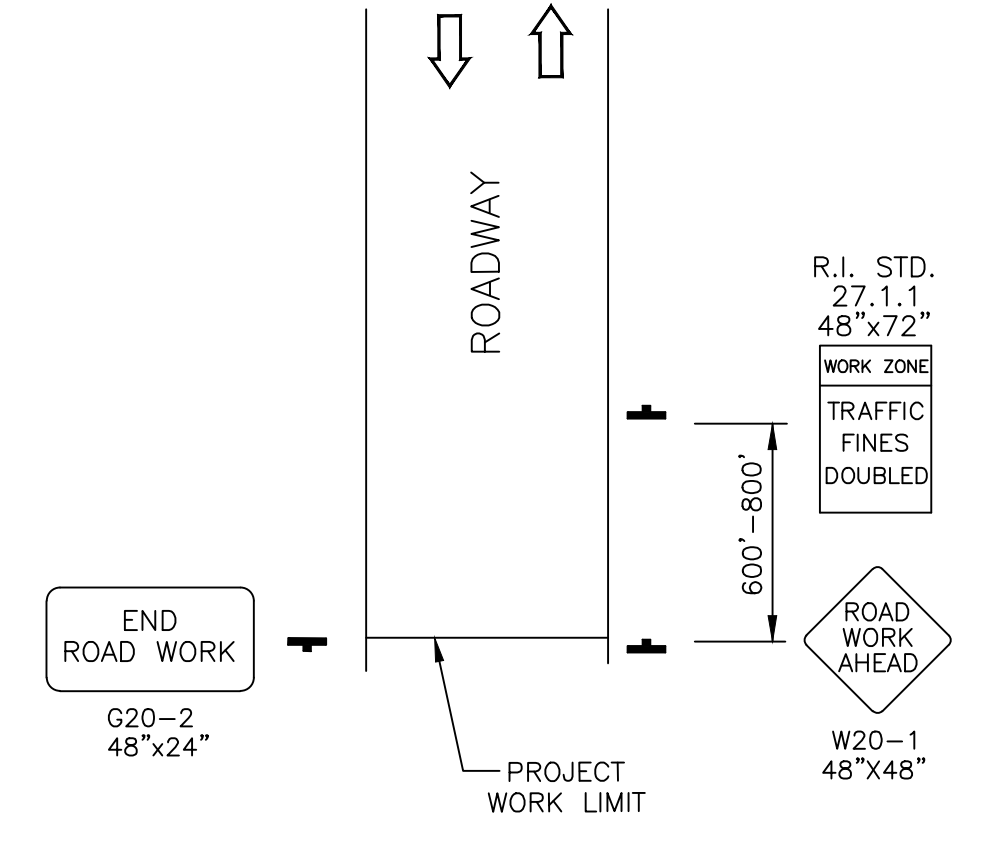
**TYPICAL LEFT SHOULDER CLOSURE ON A FREEWAY**



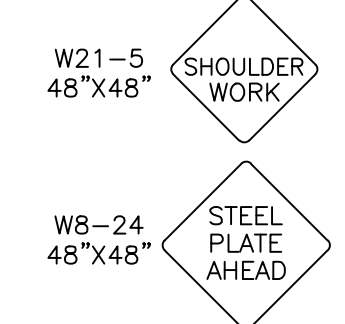
**NOTES:**

1. THIS INFORMATION ALSO SHALL BE USED WHEN WORK IS BEING PERFORMED IN THE LANE ADJACENT TO THE MEDIAN ON A DIVIDED HIGHWAY. IN THIS CASE, THE LEFT LANE CLOSED SIGNS AND THE CORRESPONDING LANE ENDS SIGN SHALL BE SUBSTITUTED.
2. WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TTC ZONE, ADDITIONAL TTC DEVICES SHALL BE PLACED AS NEEDED.

**TYPICAL LANE CLOSURE**



**TYPICAL PROJECT LIMIT DETAIL**



**MISCELLANEOUS USE SIGNS**

**NOTES:**

1. IF STEEL PLATES ARE USED A W8-24 SIGN SHALL BE PLACED IN ADVANCE ACCORDINGLY.



RHODE ISLAND  
TURNPIKE AND BRIDGE AUTHORITY

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

**BRIDGE REPAIRS**  
JAMESTOWN - VERRAZZANO BRIDGE NO. 080001 - TRESTLE STRUCTURE  
FISHING PIER ACCESS ROAD BRIDGE NO. 083901  
NORTH KINGSTOWN RHODE ISLAND  
**MAINTENANCE AND PROTECTION OF TRAFFIC DETAILS - 2**